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Navy News

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KUWAIT WAS 'COPY-BOOK'

ACTION Commandos there within 24 hours DEMONSTRATION OF NAVY'S MOBILITY

If any proof were needed of the Navy's undiminished role in our defence system, it can be seen in the recent operations at Kuwait. The ability of the Navy to concentrate forces speedily and effectively has seldom been better demonstrated and it is hoped that the lesson has not been lost, not only on those responsible for the higher direction of affairs, but on the great mass of the public on whom, ultimately, the support of the Navy depends.

The recently concluded defence agreement between the ruler of Kuwait and Her Majesty's Government, and the announcement of Kuwait's intention to apply for membership of the United Nations, were used by General Kassem as the occasion to announce his intention of annexing this small but wealthy oil sheikdom, which produces about 40 per cent. of our oil. This called for immediate steps on our part to alert the necessary forces and to make preparations for giving military assistance to the ruler should he require it. This proved in fact to be the case. On June 30 the ruler of Kuwait asked Britain for forces to enter Kuwait and the Commander-in-Chief, Middle East, was instructed to begin landing on the following day.

Already at Bahrain were H.M.S. Loch Alvie and the Amphibious Warfare Squadron, H.M.S. Striker having half a squadron of Centurion tanks embarked, which she was due to exchange with those carried in the Army's landing ship, Empire Gull. In dock at Karachi was H.M.S. Loch Fyne, whilst H.M.S. Loch Ruthven was at Mombasa.

H.M.S. Bulwark, with No. 42 Commando embarked, was about to visit Karachi before proceeding to the Persian Gulf for hot-weather trials. She was thereupon sailed from Karachi with all dispatch in order to arrive at Bahrain on July 1, while the Loch Ruthven sailed from Mombasa for the Persian Gulf. The Loch Fyne was ordered to cut short her docking and self-maintenance, and with the assistance of the Pakistan Navy, succeeded in sailing from Karachi on July 1, ten days ahead of schedule.

Meanwhile the aircraft carrier Victorious, with the destroyer Cassandra and the frigate Lincoln in company, were approaching Hong Kong. These were turned round and routed to the Persian Gulf at 22 knots. Towards the end of the voyage the Cassandra was sent on ahead and thus became the first ship from the Far East to enter the Persian Gulf. H.M.S. Yarmouth and H.M.S. Llandaff were sailed from Hong Kong to join the Victorious, and her replenishment group were ordered to follow up.

FLEW FLAG IN SUBMARINE HE ONCE COMMANDED

H.M.S. Trenchant, first commissioned in February, 1944, paid off on July 31 prior to being scrapped.

For her final dive on July 12, the Flag Officer Submarines, Rear-Admiral A. R. Hezlet, C.B., D.S.O. and Bar, D.S.C., who commanded Trenchant in the Far East at the end of the war, was embarked, and when she entered Haslar Creek for the last time she flew his flag.

During her period of active service Trenchant sank a total of 27 ships including the Japanese heavy cruiser Haguro and a German U-boat.

Since the war she has served on the Home and Mediterranean stations. She has been fitted with snort and has had three external tubes removed, but is otherwise unchanged since her wartime service.

*Welcome to
Navy Days
The Navy
in Pictures
pages 8 & 9*

H.M.S. Centaur, accompanied by the destroyers Solebay, Finisterre, Camperdown and the R.F.A. Olna were sailed from Gibraltar, passing through the Suez Canal without incident on the night of July 5. H.M.S. Loch Insh, with two minesweepers (H.M.S. Ashton and H.M.S. Rodington) and H.M.S. Messina, followed three days later.

The mining threat could not be ignored and it was decided to sail 108th Minesweeping Squadron from the Mediterranean.

FIRST WAVE ASHORE

The fact that the Bulwark was so readily available enabled 42 Commando to be put ashore in Kuwait inside 24 hours of the ruler's request for help. The following day 45 Commando was flown up from Aden, so the Commandos were properly in the "first wave." This was particularly valuable because of difficulties which hampered flying in reinforcements from outside the theatre. The build-up to a balanced force of approximately 5,700 men was completed in nine days and was considered a very satisfactory achievement in the circumstances.

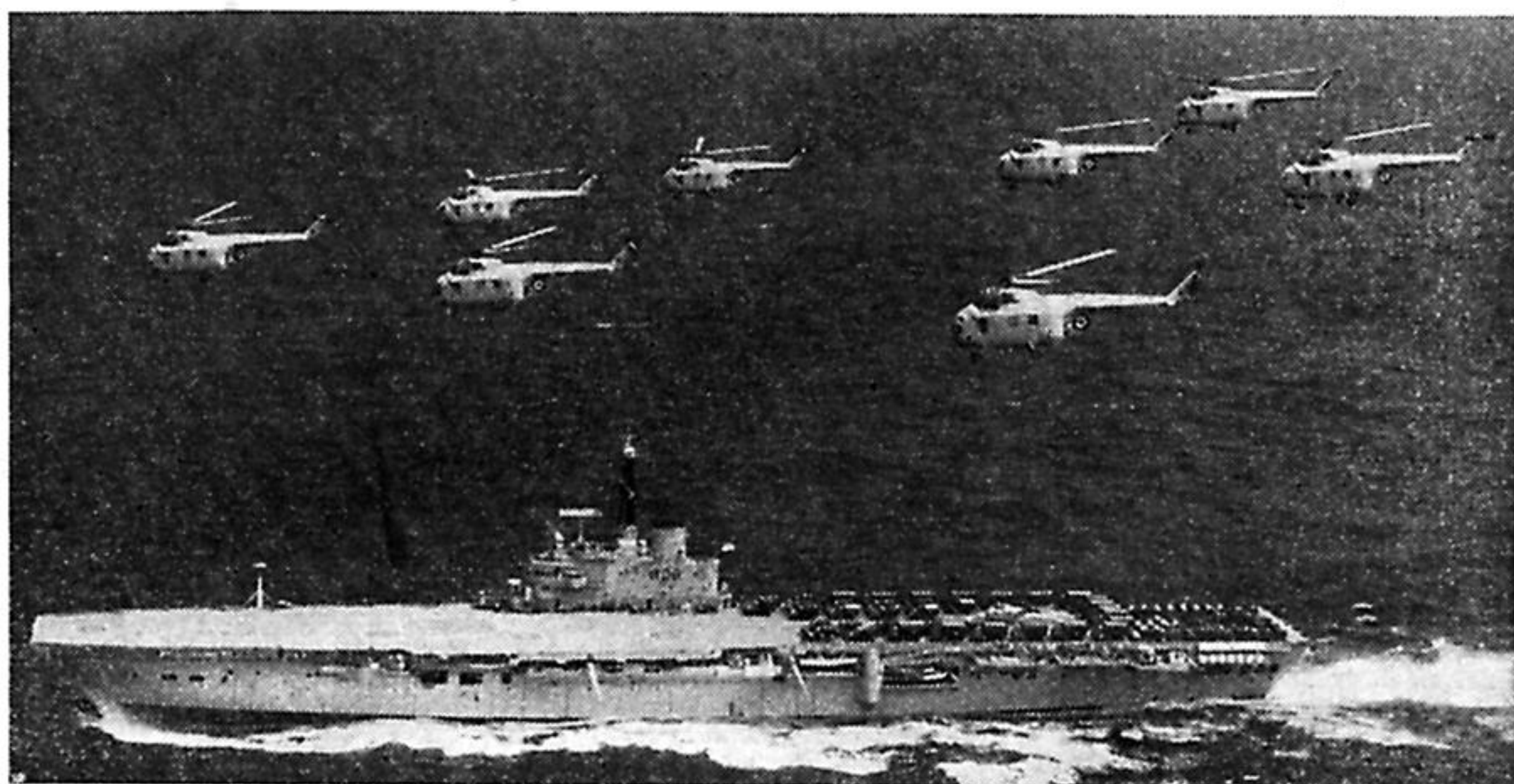
The duties of Headquarters Ship and communications link with the land forces was carried out throughout the operation by H.M.S. Meon.

THE POPULAR BULWARK

The air-conditioned facilities in H.M.S. Bulwark added much to her popularity, and some 200 men daily were sent on board for a clean-up and rest. The ship also acted as the early warning radar guard.

A noteworthy feature of the operation was that H.M.S. Victorious assumed full air defence responsibility on arrival in the Gulf, controlling both her own aircraft and the R.A.F. Hunters from Kuwait Airfield. This was the first occasion that Type 984 radar equipment has been used operationally.

(Continued on page 3)



Helicopters flying above H.M.S. Bulwark

400 to be freed from shore-base jobs at Devonport CIVILIANS TO REPLACE 140

In order to ensure that the reformed manpower of the Royal Navy is kept deployed to best effect, the tasks and complements of shore establishments are regularly reviewed. Such a review has been completed at the Royal Naval Barracks, Devonport.

The centralised drafting system inaugurated four years ago has proved most effective in reducing the number of men previously held there in transit and waiting draft. Other developments have made possible the re-allocation of certain training tasks. This will lead to a further reduction in the number of

men borne in barracks. The various steps will take effect during the next two years, and reduction in administrative services will follow automatically.

It is also intended to replace 140 naval ratings by civilians for various maintenance, domestic and administrative services which will still be required.

In all, the changes will free some 400 or more naval personnel from shore based jobs in Devonport.

The prime tasks of H.M.S. Drake in future will be to serve as an Accommodation Centre for naval personnel in transit and for certain ships refitting in Devonport dockyard; and to act as an accounting centre for those ships and establishments of the Plymouth Command which do not carry their own accounts.

Members of W.R.N.S. will continue to serve at Devonport and will perform the same tasks as at present but on a somewhat reduced scale. Consideration is being given to the possibility of finding them accommodation within H.M.S. Drake to enable their present quarters at St. Budeaux to be closed.

The possibility of the reduction of certain of the tasks in the R.N. Barracks Portsmouth is under review, but circumstances there are somewhat different from Devonport.

Twenty-ninth submarine since war

H.M. Submarine Sealion, the eighth boat of the Porpoise class and the 29th submarine to be completed for the Royal Navy since the Second World War, was accepted for service on July 25.

Built by Cammell Laird & Co. (Shipbuilders and Engineers), Ltd., the Sealion was launched in December, 1959, by Lady Dalton, wife of Vice-Admiral Sir Norman E. Dalton, K.C.B., O.B.E.

Boats of this class are 293 feet 3 inches in length and have a beam of 26 feet 6 inches. They are powered by diesel-electric drive from Admiralty Standard Range engines which give a high under-water speed. They are air-conditioned for service in both tropical and cold climates, and the ability to carry large quantities of all types of stores enables them to remain on patrol for months without any outside support.

The Sealion's complement of six officers and 62 ratings will have amenities of the highest possible standards to help combat the boredom of long submerged patrols.

Under the command of Lieut.-Cdr. A. N. Derrick, R.N., the Sealion will serve with the Third Submarine Squadron based on the Gareloch, Scotland.

The other boats of this class already in service are the Porpoise, Rorqual, Narwhal, Grampus, Cachalot, Fin-whale and Walrus.

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Navy News

EDITOR

Lieut. (S) H. R. Berridge, R.N. (Retd.),
Royal Naval Barracks, Portsmouth
Tel.: Portsmouth 22351 (Ext. 72194)

EDITORIAL

SO far as the Royal Navy is concerned the heat, the sand, the flies and the stress of the Kuwait affair is a thing of the past. There is no doubt that the sympathies of the Royal Navy and Royal Marine officers and men who took part in the landings, at the request of the Kuwait Government, are with those still there doing a job of work, and those there can rest assured that the Royal Navy is not too far away should its services be required.

If anything was needed to justify the Commando Carrier the Kuwait affair has done it. Within a very few hours of the request of the Kuwait authorities, H.M.S. Bulwark was on its way. Without going into politics it seems perfectly obvious that the timely arrival of the carrier with its Royal Marine Commandos and helicopters prevented a clash of arms which might have led to a much bigger conflagration.

It was for just such an occasion that the Bulwark was converted to a Commando Carrier—to deal with a "bush fire" before it could develop into something bigger. When Albion, which is being similarly converted, is ready, the Royal Navy will be in a position to maintain one in each of the western and eastern fleets, but as ships need to be refitted and modernised, two such ships are insufficient. One more at least is required.

It would not be surprising if the Admiralty was to find the money necessary for another "fire fighter" for, although Bulwark was soon "on the spot," the Victorious and Centaur were quickly on the way, the distances involved are so great that a similar outbreak in the future might find the ships not so fortuitously placed as they were on this occasion.

The efficiency with which Bulwark dealt with her task in terribly difficult conditions has earned high praise, and showed that the training the officers and men have received in various parts of the world, including the recent combined amphibious exercise involving all three services 90 miles from Aden in the Western Aden Protectorate, stood them in very good stead.

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H.R.H. the Duke of Edinburgh watches the Surf Life-Saving Team in action during his visit to H.M.S. Fisgard last month

ROYAL VISITOR TO FISGARD

INFORMALITY was the keynote of the occasion when H.R.H. the Duke of Edinburgh visited H.M.S. Fisgard, the Apprentices (Part 1) Training Establishment at Torpoint on June 5.

In an hour the Duke visited the "Exped" room, saw the apprentices at work in the Factory and witnessed a display in the Swimming Bath.

The Surf Life Saving Club carried out a "dry run" displaying the ceremonial drill procedure—the "patient" running about 40 yards from the reel while Number two man laid the belt at the ready. On a given word the whole team sprang into action, ending by giving a demonstration of the Holger Neilson method of artificial respiration.

An explanation of the mouth to nose resuscitation was given to H.R.H. by Apprentice Jones.

New assembly hall

IT is hoped that the foundation stone of a new Assembly Hall for H.M.S. Fisgard will be laid this year. The site chosen is on the western flank of the Parade Ground necessitating the demolition of the Decontamination Centre and the shelters.

The large building, handy for indoor divisions, will be a multi-purpose hall, containing a full size stage with dressing rooms at one end and an Altar and Sanctuary at the other end so that Church can be held there on Sundays. Folding doors will screen the Sanctuary in the week.

DRAFTING FORECAST—YOUR NEXT SHIP

Notes: (i) The term U.K. Base Port means the port at which a ship may normally be expected to give leave and refit. Portsmouth (C) indicates ships administered by Portsmouth but which will normally refit and/or give leave at Chatham.
(ii) As ratings are normally detailed for overseas service about four months ahead of commissioning date, and for home service about two months ahead of commissioning date, this should be borne in mind when preferring requests to volunteer to serve in a particular ship.
(iii) It is emphasised that the dates and particulars given below are forecasts only and may have to be changed—perhaps at short notice.

SUBMARINE COMMAND

H.M.S. Sealion, July 25, at Birkenhead, for service in Third Submarine Squadron.
H.M.S. Acheron, September, at Devonport, for service in Second Submarine Squadron.
H.M.S. Rorqual, end of October, at Devonport, for service in First Submarine Squadron.
H.M.S. Andrew, December 16, at Singapore, for service in Seventh Submarine Division.
H.M.S. Aeneas, January, 1962, at Portsmouth, for service in Second Submarine Squadron.
H.M.S. Tabard, February 2, at Sydney, N.S.W., for service in Fourth Submarine Division.
H.M.S. Artemis, end of February, at Chatham, for service in Second Submarine Squadron.
H.M.S. Thermopylae, March 2, at Chatham, for service in Fifth Submarine Division.
H.M.S. Grampus, April, at Portsmouth, for service in First Submarine Squadron.
H.M.S. Auriga, May 18, at Devonport, for service in Second Submarine Squadron.

GENERAL

H.M.S. Blake, August changes to General Service Commission Med./Home (24 months) U.K. Base Port, Devonport.
H.M.S. Vidal, August 9, at Chatham for trials, Commissions September 12 for General Service Commission West Indies (24 months). U.K. Base Port, Portsmouth (C). (See note.)
H.M.S. Alert, August 10, at Singapore for Foreign Service (Far East).
H.M.S. Nimble, August 11, at Devonport for Home Sea Service (Steaming Crew).
H.M.S. Puma, August 22, at Devonport for General Service Commission Home/South America and South Atlantic (20 months) U.K. Base Port, Devonport.
H.M.S. Berwick, August 28, changes to General Service Commission, Home/Med. (19 months). U.K. Base Port, Portsmouth.
H.M.S. Virago, September, at Devonport for Home Sea Service. U.K. Base Port, Devonport.

H.M.S. Bulwark, September 5, at Singapore for Foreign Service (Far East).
H.M.S. Dundas, September 5, at Rosyth for trials, Commissions October 31 for Home Sea Service. U.K. Base Port, Devonport.
H.M.S. Ark Royal, September 12, at Devonport for General Service Commission, Home/Med. (24 months) U.K. Base Port, Devonport.
H.M.S. Dundas, September 14, at Rosyth for trials, Commissions October 31 for Home Sea Service. U.K. Base Port, Devonport.
H.M.S. Corunna, September 21, at Rosyth for trials, Commissions November 30 for General Service Commission, Med./Home (24 months) U.K. Base Port, Rosyth.
H.M.S. Lowestoft, September 26, at Glasgow for Home Sea Service. Commissions for General Service Commission, Med./Home (16 months) December, U.K. Base Port, Portsmouth (C). (See note.)
H.M.S. Loch Ruthven, September 27, at Rosyth for General Service Commission, Home/Middle East (18 months). U.K. Base Port, Devonport.
H.M.S. Brighton, September 27, at Glasgow for Home Sea Service. Commissions April, 1962, for General Service Commission East of Suez/Home (21 months) U.K. Base Port, Portsmouth.
H.M.S. Hardy, September 28, at Chatham for trials, Commissions December 5 for Home Sea Service. U.K. Base Port, Devonport.
H.M.S. Hardy, October 3, at Chatham for trials, Commissions December 5 for Home Sea Service. U.K. Base Port, Devonport.
819 Squadron, October 4, at R.N. Air Station, Eglinton Re-equipping.
H.M.S. Aisne, October 10, at Chatham for trials, Commissions January 9 for General Service Commission Med./Home (24 months). U.K. Base Port, Portsmouth.
H.M.S. Ashanti, October 31, at Glasgow for Home Sea Service (Trials). April 1962 changes to General Service Commission Middle East/Home (12 months). U.K. Base Port, Devonport.

Letters to the Editor

RIGGING OF BRIGS

SIR.—Rear-Admiral Blackman, of Peak Cottage, Near Bishops Waltham, Hampshire, England, recently learned that I was seeking information on the construction and rigging of the Royal Navy training brigs which were in service around the turn of the century.

Admiral Blackman kindly suggested that I write you, saying he believed that you might have space to publish a short request for details of these vessels. If you could do so, it would be greatly appreciated.

I shall be glad to hear from anyone who can furnish information on the training brigs. Close-up photographs will be very helpful, especially ones showing details of bows and sterns, deck furniture and the lead of rigging. Of course, I expect to pay for these pictures. Thank you very much.—ROY ROGERS, 5001-14th Avenue, Apt. C-6, Brooklyn 19, New York.

EDMOND HALLEY AND MARTIN VAZ ISLAND

SIR.—It was with great interest that I read the account of the very difficult landing on Martin Vaz Island in the South Atlantic and the scientific investigations of the naval men and scientists from H.M.S. Owen.

I have been recently studying accounts of the naval service of the astronomer Edmond Halley. For three years he commanded H.M.S. Paramour, which was employed on research to improve navigational methods. On April 14, 1700, he anchored off the Isle of Trinidad, in the Martin Vaz group, now Brazilian.

He landed, planted the Union Flag, took possession of the island for King

William III and put ashore goats, pigs and fowls that he had brought from St. Helena. Supplies of meat and fresh water were taken aboard the Paramour.

Halley sketched the appearance of the main island, Martin Vaz, but it appears that he did not land upon its rugged shore.

I am very grateful to Miss S. L. Fisher, of the Greenwich Maritime Museum, for verification of this information.—JOHN DICKSON, 153 Kingsway, Hove, 3.

ROUGH WEATHER COULD NOT SPOIL GARDEN PARTY

ALTHOUGH rough weather drove them indoors, the H.M.S. Vernon branch of the R.N. Friendly Union of Sailors' Wives had a "full house" for their garden party on July 12.

The cinema was used for the main part of the afternoon's programme of sideshows, roundabouts and swing-boats (for the younger element!) and an ankle-judging competition. This was judged by Commander J. Blake, R.N., and Second-Officer R. Harris, W.R.N.S., and was won by Mesdames Weaver and Bradlaugh from a large number of contestants.

The company then proceeded to the Petty Officers' Mess (very kindly lent for the occasion) for tea.

The guest of honour, Lady Power, wife of the Commander-in-Chief, Portsmouth, presented prizes to Mesdames Buch, Weaver, Bradlaugh, Dale, Cauldwell and Quain.

Previously given a bouquet by Susan Jay, Lady Power was also presented with a farewell gift from the branch at the end of the afternoon.

14 Squadron, December, at R.N. Air Station, Culdrose, for Overseas Service. (H.M.S. Hermes.)
H.M.S. Hermes, December, at Portsmouth, for General Service Commission, Home/East of Suez (24 months). U.K. Base Port, Portsmouth.
H.M.S. Cavalier, December, at Singapore for Foreign Service (Far East).
H.M.S. Ulster, December 14, at Devonport for General Service Commission West Indies/Home (21 months) U.K. Base Port, Devonport.

1962

H.M.S. Loch Fyne, January, at Devonport, for General Service Commission, Home/Middle East (15 months). U.K. Base Port, Devonport.
H.M.S. Barrosa, January 5, at Devonport for trials, Commissions April 3. U.K. Base Port, Portsmouth (C). (See note.)
H.M.S. Hermes, January 30, at Portsmouth, for General Service Commission, Home/Med. (24 months) U.K. Base Port, Portsmouth.
809 Squadron, March, at R.N. Air Station, Lossiemouth.
H.M.S. Devonshire, February, at Birkenhead for Home Sea Service Commission for General Service Commission, July Home/Med. (24 months) U.K. Base Port, Portsmouth.
H.M.S. Agincourt, February 13, at Portsmouth for trials Commission May 1 for General Service Commission Home/Med. (24 months). U.K. Base Port, Portsmouth.
847 Squadron, March 6, at R.N. Air Station, Culdrose, for Foreign Service.
H.M.S. Rothesay, March, conversion to Home Sea Service for London-derry Squadron. U.K. Base Port, Portsmouth.
H.M.S. Rame Head, March, at Chatham, for trials.
H.M.S. Caprice, March, at Singapore for Foreign Service (Far East).

H.M.S. Gurkha, March, at Southampton, for Home Sea Service. Commissions for General Service Commission May, Middle East/Home (18 months). U.K. Base Port, Rosyth.

801 Squadron, March, at R.N. Air Station, Lossiemouth, for Overseas Service.

H.M.S. Nubian, end March, at Portsmouth for trials, Commissions for Home Sea Service September, General Service Commission Middle East/Home, October, 1962 (18 months). U.K. Base Port, Portsmouth.

H.M.S. Whirlwind, April, at Chatham for General Service Commission Home/W. Indies (24 months). U.K. Base Port, Portsmouth (C).

H.M.S. Cassandra, April, at Singapore, for Foreign Service (Far East).

H.M.S. Yarmouth, April, at Devonport, for General Service Commission, East of Suez/Home (18 months). U.K. Base Port, Devonport.

H.M.S. Blackpool, April, at Chatham, for General Service Commission, East of Suez/Home (15 months). U.K. Base Port, Portsmouth (C). (See Note.)

H.M.S. Llandaff, April, at Devonport, for General Service Commission, East of Suez/Home (20 months). U.K. Base Port, Devonport.

H.M.S. Loch Alvie, April, at Chatham, for General Service Commission, Middle East/Home (18 months). U.K. Base Port, Portsmouth (C). (See Note.)

H.M.S. Tartar, April, at Devonport for trials, Commissions November for General Service Commission Middle East/Home (18 months) U.K. Base Port, Devonport.

H.M.S. Maidstone, end of April, at Portsmouth for trials, Steaming Crew, Home Sea Service. U.K. Base Port, Rosyth.

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Five 'Saunas' in four days

H.M.S. Diana visited Turku (Abo), the second city and ancient capital of Finland from June 28 to July 2 and the experience was one which would not have been missed, willingly, by any of those on board.

On the first day, after the paying and receiving of official calls, the Captain (Captain G. J. Kirkby, D.S.C., R.N.) laid a wreath on the Finnish Naval War Memorial at a simple ceremony attended by a guard from the ship and the band of the Finnish Navy.

A large amount of official and private hospitality was offered and visits arranged for the ship's company included trips to an oil refinery, an ice-cream factory (where unlimited free ice cream was provided), a shipyard, a pottery and a brewery.

The party attending the brewery walked there but transport was provided back, because 500 bottles of beer were provided for 36 ratings! They were, however, unable to drink it all in the time available and returned on board in good order.

During the stay the ship lost to the Finnish Naval Base 2-1 at soccer and were also narrowly beaten in a swimming relay.

The ship was open to visitors on two days and 3,800 people came on board, and a total of 150 Finnish officers and ratings made conducted tours of the ship. A children's party for 40 orphans was also held on board.

THE SAUNA

Many of the ship's company were initiated into the "sauna." This begins with a long sit in a room heated to about boiling point, followed by a flying leap into a lake or swimming pool at around 60 degrees F. One officer survived five saunas in four days; surprisingly there were no heart attacks.

H.M.S. Diana sailed from Turku at 1900 hours on Sunday, July 2. The ship was played out of harbour by the Finnish Naval Band playing "Land of Hope and Glory" to round off a most enjoyable visit.

KUWAIT WAS 'COPY BOOK' ACTION

(Continued from page 1)

THE RUN-DOWN

Certain naval units, after operating in support of Kuwait for three weeks in the world's hottest sea, are now being withdrawn to cooler waters. The first to leave is the destroyer Cassandra, who will have been at sea continuously for eight weeks. Soon after, the carrier Victorious and her escorts, Lincoln, Yarmouth, Finisterre and Loch Fyne, leave. Most of the early arrivals were from the Far East and will be replaced by ships drawn from the Mediterranean and Home Stations.

Far East ships steamed 4,500 miles to get to the Persian Gulf, and Victorious has won the distance championship with 12,000 logged in the operation. Fuelling, feeding and storing this force amounting to 3,500 sailors has been the job of the under-way replenishment ships, the R.F.A.s. Tidereach, Reliant and Resurgent, who have had to provide over 600 tons of fuel, stores and provisions for each day of the operation.

LIVING CONDITIONS

Climatic conditions have made life in all ships unpleasant, despite air conditioning, which has been vital in operating delicate electronic equip-

ment on board. Living and sleeping quarters averaged 95-100°F. and machinery spaces up to 130°F. The high humidity, over 90 per cent, soon after sunrise, completed the heat discomfort. Even at night little respite was gained due to the high sea-water temperatures and effect of the sun beating down on steel decks all day. Everyone rose to the challenge of these unpleasant conditions and morale was high and health good.

Much ingenuity was shown in alleviating these hardships. Ships' routines were adjusted so that work was done in the cool dawn periods. Thirst-quenching barrels of iced water and lime juice were strategically placed. Formality in dress was relaxed and men went near naked about their work. Salt-water showers and canvas baths were rigged on the upper deck so that men could refresh themselves without wasting fresh water.

Other factors which supported morale were the regular air-mail deliveries three to five days only from home, ample film supplies, and opportunities for impromptu sport such as volley ball, tug-of-war, deck hockey and rifle shooting. It was not a spectacular job, but ships have been busy. Victorious kept her aircraft in flying practice day and night, ready to support the force ashore in Kuwait at any moment should Kassem decide to invade the sheikdom of Kuwait.

THE SMALL SHIPS

The small ships, the maids of all work, were ceaselessly active, guarding the carrier, carrying despatches, stores and mail, protecting the approaches to Kuwait, searching dhows and preparing to support with naval gunfire their comrades in arms ashore.

SPIRIT IN THE FLEET

The spirit in the fleet is of work well done for a worth-while end under difficult conditions, but ready for a stand-down so that, if required, they may return refreshed and revitalised for other patrols.

As a practical demonstration of the Navy's versatility and mobility, there is no doubt that this operation has been a great success.

SHIPS OF THE ROYAL NAVY

H.M.S. Blake

No. 69



Advancements

CONFIRMATION has been received that the following have been advanced to the Chief Petty Officer or Chief Artificer rate:

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MX79255 R. D. Freeman.

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MX742123 P. H. Boyce, MX904795 L. J. Howell, MX59158 T. Howard, MX878781 D. J. Dixon, MX871582 K. F. Lewis, MX834361 E. V. C. Taylor, MX650041 T. J. Parr.

TO STORES CHIEF PETTY OFFICER (S)

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TO CHIEF PETTY OFFICER COOK (S)

MX857919 W. A. Dinwoodie, MX107637 A. Sheaff, MX61637 M. H. Goddard.

TO CHIEF PETTY OFFICER COOK (O)

MX800749 N. W. Boyne, MX666720 C. Shaw.

TO CHIEF RADIO COMMUNICATION SUPERVISOR

JX371973 K. A. Smith, JX712918 D. W. C. Foote, JX646409 A. D. Duffin, JX853282 A. T. S. Perry, JX646313 W. G. Wharton, JX371698 P. J. Evans.

TO CHIEF COMMUNICATION YEOMAN

JX612435 W. W. J. Taylor, JX292888 J. Kelly, JX581496 R. E. Saunders.

TO SICK BERTH CHIEF PETTY OFFICER

MX816232 G. C. Hurst, MX65061 G. B. Williams.

TO MASTER-AT-ARMS

MX759512 J. A. Hughes.

TO ACTING CHIEF AIRCRAFT ARTIFICER (AE)

L/FX667809 R. Cunningham, L/FX669830 D. B. Wyatt, L/FX513019 P. W. Craig.

TO ACTING CHIEF AIRCRAFT ARTIFICER (O)

L/FX669262 R. W. Wood.

TO CHIEF AIR FITTER (AE)

L/FX816359 R. Lapping.

TO CHIEF AIRMAN (AI)

L/FX670531 G. F. McKenna.

TO ACTING CHIEF ELECTRICAL ARTIFICER (AIR)

L/FX888796 G. Hill.

TO ACTING CHIEF ELECTRICAL MECHANICIAN (AIR)

L/FX834292 E. F. King.

TO CHIEF ELECTRICIAN (AIR)

L/FX87162 L. C. Scott, L/FX797464 J. Williams.

TO ACTING CHIEF RADIO ELECTRICAL ARTIFICER (AIR)

L/FX857663 J. O. W. Cann.

H.M.S. Blake, the last of the three cruisers of the Tiger class, considered to be the last of the conventional cruisers, was laid down on August 17, 1942, at the Fairfield Shipbuilding and Engineering Co. Ltd., on the Clyde, the launching ceremony taking place on December 20, 1945. The naming ceremony was performed by Lady Blake, wife of Vice-Admiral Sir Geoffrey Blake.

Work on the ship was suspended in September, 1947, and the Blake was laid up at Greenock. In 1953 she was moved to the Garelock and in 1954 she was taken back to Fairfield's Yard at Govan and reconstructed to a new

design. The boilers and main engines are those originally installed but little else remains to connect this 1961 Blake with the ship laid up in 1947.

The Blake's length (o.a.) is 555 ft. 7in. with a beam of 64 ft. Her standard displacement is 9,500 tons. Her main armament is two twin 6 in. turrets, each firing 20 rounds per minute per gun, and the secondary armament is three twin 3 in. turrets, each firing 120 rounds per minute per gun.

All guns are controlled by automatic, radar-operated fire control systems capable of engaging very fast surface or air targets. Blake is able to fire a total weight of shell of several tons a minute.

All main machinery is capable of automatic operation and can be remotely controlled.

This class of cruiser has been designed to provide close cover and anti-aircraft support for convoys and aircraft carrier groups and to provide support for assault landings, while preserving the versatility to fulfil a number of other peace-time roles.

FIRST LORD AT DARTMOUTH

THE First Lord of the Admiralty (The Rt. Hon. Lord Carrington, K.C.M.G., M.C.) took the salute at the passing-out parade of Midshipmen at Britannia Royal Naval College, Dartmouth, on July 31.



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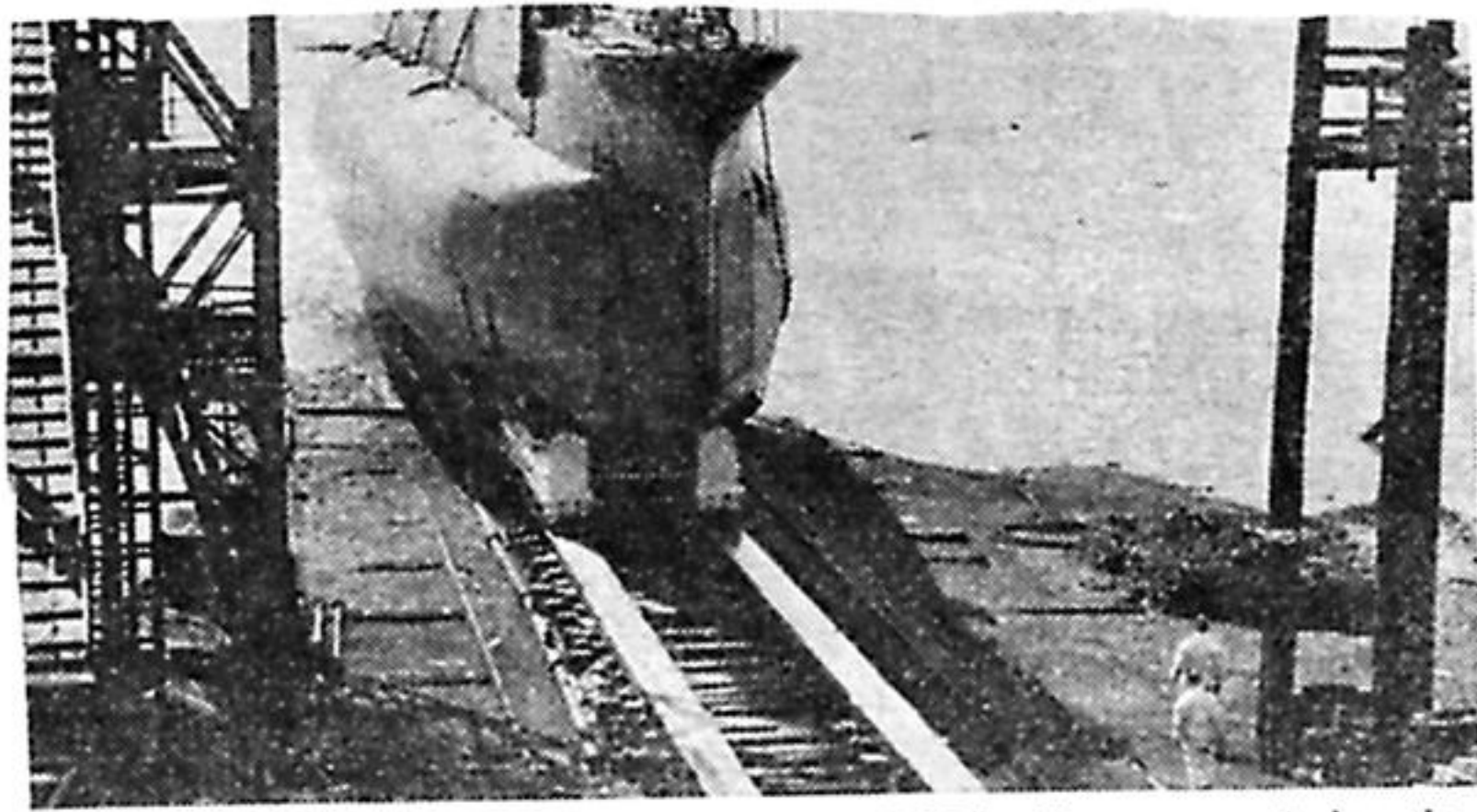
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The sixth submarine of the Oberon class—H.M.S. Olympus—entering the water at Barrow, on June 14, after her launching and naming by Mrs. R. H. Wright, wife of Vice-Admiral R. H. Wright, C.B., D.S.C., the Flag Officer, Scotland.

Have YOU a personal problem...? ASK JOHN ENGLISH

John English will be pleased to answer your queries. A stamped addressed envelope will be appreciated.

Men on General Service Commission are lucky

ALTHOUGH a youngster's first commission abroad may be regarded as a "holiday cruise" (see the story on this page), when men get married and have more responsibilities, the holiday cruise becomes "another job of work." The average sailor, realising he is part of the Senior Service, takes his time abroad in his stride. His wife, however, perhaps living among comparative strangers, is apt, sometimes, to feel that her husband is not getting a fair crack of the whip.

An understanding wife is the key to a happy marriage and once a wife grasps the idea that Naval Drafting is carried out with utmost care and impartiality — that there are no "fiddles" — then the wife, like the husband, will accept the inevitable disruption.

The article by Archibee in the June issue of "Navy News" aroused more interest than anticipated and it is obvious that the present day sailor has a sound knowledge of the complex organisation of the drafting world.

Home Sea Service, Overseas Service, Port Service—all these are terms which everyone understands and has a good idea when he is due for them. The one form of service which does seem more difficult to grasp is perhaps the General Service Commission.

This, together with Foreign Service and Local Foreign Service, makes up the three forms of Overseas Service.

LUCKY MEN

In many ways the chap who is detailed to a General Service Commission ship is a lucky man. The ship may refit in a U.K. port and some of the period of the General Service Commission is probably spent in U.K. ports. You may therefore often get home from time to time, whereas you cannot normally do this on Foreign Service or Local Foreign Service. Despite this, the General Service Commission is often wrongly abused and looked upon as the Cinderella of this form of Service.

I met an M.E. the other day who could not understand why he was in a General Service Commission ship refitting in Devonport, when his Preference Area was Portsmouth. Although not a Drafting expert I think I was able to explain the reason to him and he left a happier man. I

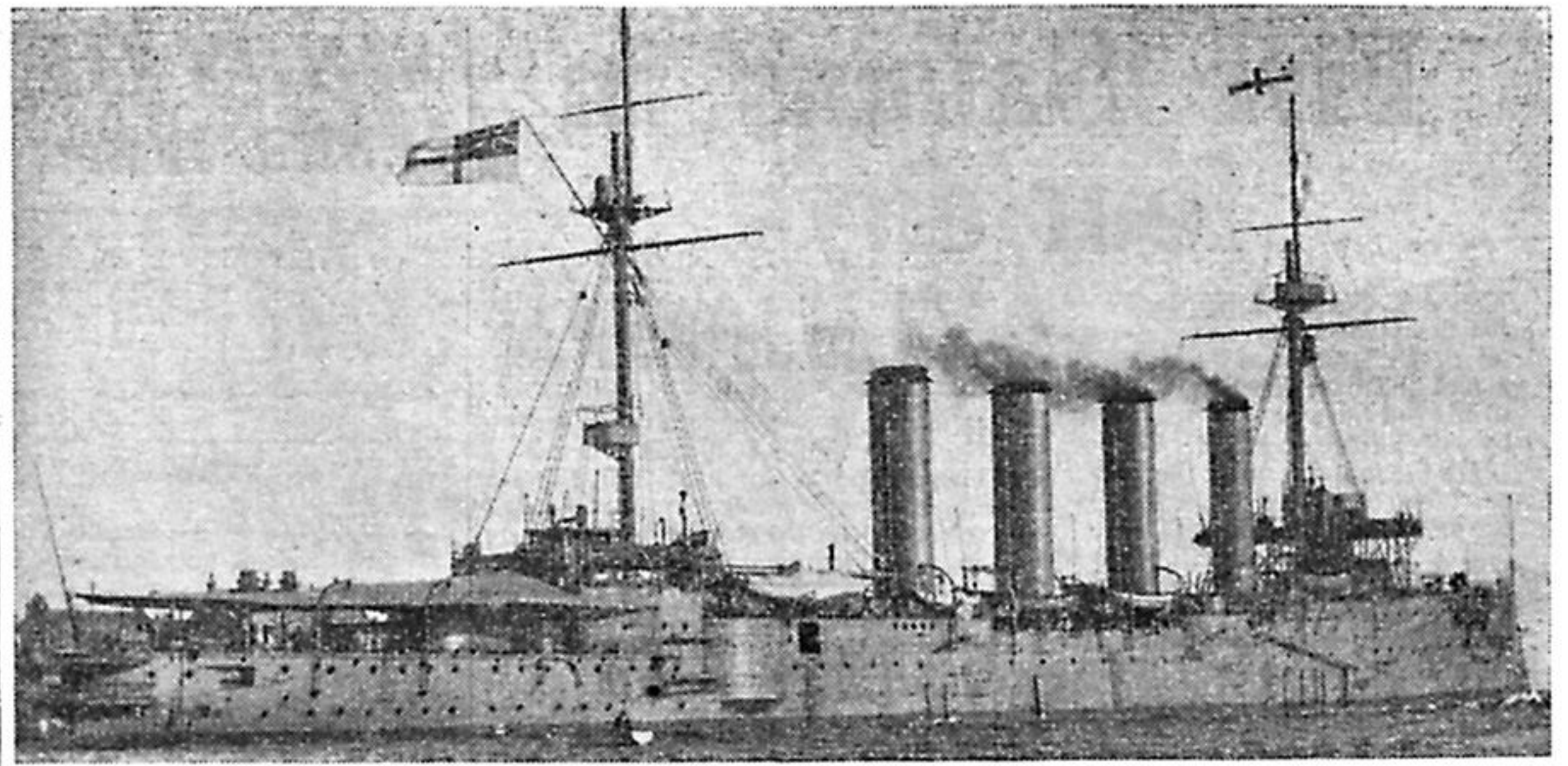
wonder how many more chaps labour under the same sort of delusion.

A 'G.S.C.' IS 'OVERSEAS SERVICE'

The main point is a General Service Commission is counted as Overseas Service and "Drafty" cannot therefore give as much consideration to your Preference Area as he does when drafting you to Home Sea Service or Port Service. I do know, however, that when a ship commissions with, say, Portsmouth as the Base Port, as far as possible it is manned with men who have chosen Portsmouth as their Preference Area. This cannot, however, always be arranged because often men with other Preference Areas are top of the roster for Overseas Service, and drafting to Overseas Service must be from the top of the roster or the whole basis of fair drafting will be prejudiced.

When you are serving on a General Service Commission and find yourself in a ship with Devonport as its Base Port and refitting there, while you are living in Portsmouth, spare a thought that you are on Overseas Service and are jolly lucky to be able to get home at all. Don't forget, provided you are likely to be in the ship order six months (which is practically always the case) you can apply for married quarters at the Base Port and qualify for disturbance allowance if you move there.

If the wife or girl friend feels that men on General Service Commissions don't get home often enough, a couple of minutes explaining this to her when you next have a few moments at home, will be well worth while. After all, we joined the Navy to go to sea and are very lucky or unlucky (whichever way you look at it) to get as much shore time as we do, no matter what specialisation we are in!



H.M.S. King Alfred—flagship of the China station—1905

First commission looked on as a holiday cruise

MAN-HANDLING 2,500 TONS OF COAL

(In his previous articles NEPTUNE has told how he joined the Royal Navy in October, 1904, the poor meals, poor food and rigid discipline, the "rum gluttons" and gambling and that tactless order "On the knee" which eventually led to the troubles in R.N. Barracks, Portsmouth, in 1906.)

JANUARY 2, 1906, was the morning we youngsters had looked forward to with eagerness and enthusiasm. What more could we have wished for by way of a New Year award? To commission a Flagship, destined in the near future to sail for that land of fantasy—China. . . . To sail some 10,000 miles overseas to a land which was full of mystery and intrigue, where the diplomats of the "great powers" vied with each other in their efforts to gain some advantage for their countries by acquiring some additional concession which would give them the authoritative control in rivers, harbours and seaports.

Adventure we had wanted, and at first glance at least, it looked as if our hopes were to be realised. So, on this morning of January 2, we were in high spirits, astir very early, feeling that we were about to commence a holiday. Yes, a "holiday cruise," as we dashed across the parade ground with bags and hammocks, to deposit them in the train at the barrack siding, which was to convey our baggage into the dockyard, alongside of King Alfred.

NO COMMISSION CEREMONY

The draft was duly mustered and marched into the dockyard, and brought to a halt on the jetty facing the great ship, which would be our home for two years or more. It was indeed a great moment, which filled our hearts with pride.

There was no commissioning ceremony in those days. A few words from the Commander, (Commander Consett), then our names were called, and we were handed our Commissioning Cards, on which was recorded our Part of Ship, Mess No., etc.

Then we proceeded to locate bag and hammock and take them on board to stow, and find our mess, and report to our Captain of Top.

By the afternoon, we were more or less settled in, and got down to fire drill and many other evolutions. The Dog Watches gave us time for a little reflection. I began to feel that I was of some importance. In just over a year, I had graduated from a "Northampton Rigger," to the exalted rating of Ordinary Seaman. Although feeling rather raw, it seemed that my standard was high enough to become a member of a Flagship's complement.

So here I was, being paid at the exorbitant rate of 8s. 9d. per week, but having 1s. 9d. of this large amount, retained to offset any slops I might ordered to take up, for kit maintenance. But who cared? Money was object. I at least felt that I had a disfoot on the lowest rung, of perhaps a well worn ladder, which might lead to promotion, providing I took care not to avoid those which were prone to the one down with a crash.

COALING SHIP

The day arrived when it was necessary to coal ship. Yes, 2,500 tons, were needed. The King Alfred had been berthed on the North Wall, and a collier, loaded in bulk came alongside, just a single derrick and winch each hold. This was it. For three days from dawn until well into the Dog Watches, we slogged and slogged, averaging 80-100 tons per hour. At night, cold, hungry and stiff, we had to sleep (or try to) in the hold of the collier or some odd sheltered place

inboard. What a sigh of relief went up, when the last hoist was in.

But the next problem, was, how to cleanse our bodies, from the mixture of coaldust and sweat, as there were no bathrooms, and no hot water available, except for stokers. Aggie Weston's Sailors' Rest, with its baths, washhouses, and good food were a blessing sent from heaven.

On January 31, King Alfred sailed for the China Station. The weather was good, even when crossing the Bay of Biscay. It was whilst crossing the Bay, that I did my first trick as Starboard Lookout. It was a beautiful cloudless night, with millions of stars twinkling merrily. The romance of the sea, had me fairly in its grip, so much so, that I was prepared to stay at my post for the remainder of the watch.

On reaching the Mediterranean, however, where we encountered severe weather, I lost my seagles, and anything that may have been in my stomach. The ship had a good bashing.

and proceeded to Malta for repairs. It is remarkable, how quickly one's enthusiasm returns, when reaching the sheltered water of a harbour.

Our next port of call, was Port Said, where we were coaled by native labour, and it was here, that I witnessed for the first time, the cruelty of Arab Gangers, who struck the labourer's bare shoulders with a whip, if not moving fast enough.

Then onward we went, through the then famous Suez Canal, which again brought to my mind, the slogan, "Join the Navy and see the World." Down through the Red Sea, and across the Indian Ocean, coaling at Perim and Colombo, thence to Singapore. At all ports the coaling was done by native labour.

BEAUTY AND ACTIVITY

There was great excitement as Hong Kong was approached through the channel known as the Lyman Pass. It is a narrow stretch of water, with a background of mountains on the mainland side (Kowloon), whilst on the Hong Kong side, here and there could be seen, inlets and bays which had been developed as bathing beaches.

The harbour hove into sight suddenly, revealing a most wonderful picture of beauty and activity. Ships of all nationalities were secured at buoys or jetties, either discharging or embarking merchandise. Ferries between Hong Kong and Kowloon were seen to be loaded with passengers. Tugs and steamboats in large numbers were moving rapidly about the harbour, whilst sampans and junks, with sails patched or torn were drifting in all directions. And, to complete the picture, the mountain known as the "Peak" rose in the background of the harbour and its gloriously blue water.

NEPTUNE.

(To be continued)



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SOUTHEAST'S TWO MOST



The "office sailors" team: L./Wtr. B. Tooes, of Portsmouth; L./Wtr. Gregory, of Saltash; L./Wtr. D. J. Downes, of Nottingham; Wtr. C. I of Middlesbrough; Wtr. P. Danks, of Woking; Wtr. R. Boulton, of Ellen and one other

A 'New Look' for R.N. Lay Readers

THERE is a change of considerable importance in the Royal Naval Lay Readers' Society — a change which affects the conditions under which the lay readers work — not, of course, of the work the Society does. The annual report of the President of the Society, Admiral Sir Gerald Gladstone, G.B.E., K.C.B., shows that because of the policy of not replacing stipendiary readers, a £510 deficit in 1959 was turned into a credit balance of £217 for 1960 and, in addition, the Centenary Appeal realised £683.

Over the past ten years the Society has had to realise about £6,000 capital and such a retrograde policy could not continue.

It has been decided to ask Christian laymen from within the service who are willing to help the Society in its work of bringing the challenge of Christ to those in the Royal Navy to volunteer to study in their spare time and then sit the examination set by the Central Readers' Board for a Diocesan Reader's Licence.

A team of such qualified honorary readers will be of great assistance to "the Church Afloat." They will be licensed by the Archbishop of Canterbury to conduct morning and evening prayers and to preach. As the examination is recognised by most Diocesan Bishops such honorary readers could offer their services in their home dioceses when they leave the Navy.

PAID HELPERS

The main change, however, in the Society, is that the full time paid readers of the future will be trained in the Church Army College and become officers of the Church Army, being seconded to the Society to work in the Navy.

NEW NAVAL A.D.Cs.

THE following officers have been appointed Naval Aides-de-Camp to The Queen:

Capt. G. G. Wilson, R.N., in succession to Capt. W. F. N. Gregory-Smith, D.S.O. and Bar, D.S.C. and Bar, R.N.
Capt. A. F. Black, R.N., in succession to Capt. J. V. Waterhouse, D.S.O., C.B.E., R.N.

Capt. A. J. F. Milne-Horne, R.N., in succession to Capt. (Commodore) The Viscount Kelburn, D.S.C., R.N.

Capt. L. W. Napier, D.S.O., D.S.C., R.N., in succession to Capt. (Commodore) I. F. M. Newnham, C.B.E., R.N.

Capt. C. W. Malins, D.S.O., D.S.C. and Bar, R.N., in succession to Capt. V. A. Wight-Boycott, O.B.E., D.S.C., R.N.

Capt. P. N. Buckley, D.S.O., R.N., in succession to Capt. F. M. A. Torrens-Spence, D.S.O., D.S.C., A.F.C., R.N.

Capt. T. T. Brandreth, R.N., in succession to Capt. M. P. Lawson, R.N.

Capt. J. S. Raven, R.N., in succession to Capt. K. M. Symonds, D.S.C. and Bar, R.N.

Capt. A. E. Turner, R.N., in succession to Capt. G. C. Turner, R.N.

Capt. A. L. Tapper, R.N., in succession to Capt. A. Lade, O.B.E., R.N.

The stipendiary readers would be paid by the Church Army according to the normal rate of pay of that body and participate in all other financial arrangements of the Church Army including pensions, sickness, accident and other benefits.

The R.N. Lay Readers' Society would reimburse the Church Army. Thus the Naval Readers would have a career structure and reasonable security not obtainable now.

The paid readers would wear the grey Church Army uniform with the Royal Naval Lay Readers' flashes on the jacket and overcoat. The Church

Army title "Captain" would be dropped whilst serving in the Royal Navy and the officer would be addressed as Mr. . . . Church Army.

Under the new arrangement—to be reviewed periodically—the Royal Naval Lay Readers' Society will not lose its individuality but will seek to work closely with the largest Evangelistic Society within the Church of England.

In thanking subscribers for their past contributions, the chairman of the Society said that in future one financial appeal will be made each year to raise the funds necessary to reimburse the Church Army.

ONE MAN'S MEAT IS ANOTHER MAN'S POISON

Wakeful's visit to Sweden

(BY RUDYARD OTTER)

IT is said that: "A run ashore is what you make it. . . . Depends on what you're looking for. . . . One man's meat is another man's poison," etc.

True, true, true! After spending five days from June 21 in Trelleborg, Sweden, the ship's company of H.M.S. Wakeful (Cdr. B. W. K. Hewson, R.N.) find it understandably difficult to voice a unanimous verdict on this breezy southern town near the sea. I have heard various opinions ranging from the most complimentary to the most unprintable. One thing, however, is undeniable: we did more to improve the great bonds of friendship and mutual good will between Sweden and Britain than any stuffy ministerial personage could have achieved in six months!

The Wakeful was the first British ship to visit Trelleborg since the war and, first of all, the Commanding Officer had to hold a Press conference on board. Our arrival was big news and evoked much interest throughout the country.

Later, while the officers settled down to a cocktail party with the local "bigwigs" on the quarterdeck, the ratings steamed ashore and nonchalantly gave interviews to the Press over large glasses of iced lager.

PRESS INTERVIEWS

R.E.M. Anthony Morrison saw a column or two about himself, and he presumed his photograph would attract the attention of all the flirtatious, fun-loving females in this Land of Free Love. Then during the mid-summer carnival festivities, when the population of Trelleborg congregated

in a park and held hands to dance round the musicians in the middle, and sing jolly folk songs, A.B. Brian Apps and E.M. Raymond Tookey, who were very popular with the kiddies, paused breathlessly in a sea of tiny, outstretched hands to say a few words to reporters, photographers and television cameramen.

The soccer match, too, earned us great respect from the natives, for it was a clean game, and although we lost by 6-1 there was a tremendous burst of applause from the spectators when it came to the "hip-hip-hooray" ritual. P.O./R.E.L. John Heath scored our only goal, and our players had to admit, sweetly, that the Trelleborg town team were top performers.

MIDNIGHT ANGELS

The ship was open to visitors on two days. Most of us who volunteered to show them around were invited home. Then in the evening we attended dances at the Folkets Park and the Staats Hotel, where we encountered some of the loveliest girls in the world who took a little time to break the ice but became angels later on as the clock raced towards midnight.

We also gave a children's tea party in the wardroom.

Trelleborg's rubber industry has achieved world market status, which explains why the local people are so proud of their massive factory. Many of us were invited to tour the rubber works and we came back with souvenir ashtrays—beautiful ones, partially fashioned in rubber.

Incidentally, the Trelleborg Rubber Factory also makes mouthpieces for Scottish bagpipes, so they must be doing pretty well for themselves.

Rabbits, unhappily, were generally out of the question because of the high prices, and your reporter, who is pretty hot on such matters, had to be content with a colourful postcard showing a nude female posing against a darkening sky. (Anybody got a light?)

A WRENN FOR THE WRENS

THE W.R.N.S. already have one Swan, two Nightingales and a Starling but now, believe it or not, the Wrens really have a Jenny Wren.

One of the latest recruits at the W.R.N.S. Training Establishment H.M.S. Dauntless, Burghfield, near Reading, is Wren Jennifer Rosemary Wrenn, aged 18, of Tany-y-Craig, Menai Bridge, Anglesey, who is training to become an officers' steward.

Her brother is a Royal Naval Lieutenant married to an ex-Third Officer in the W.R.N.S. Wren Jenny Wrenn now joins W.R.N.S. personnel who include (in addition to one Swan, two Nightingales and a Starling), two Finches, three Swallows, one Partridge, one Peacock, one Rook and a Raven.

As an officers' steward Wren Wrenn could find herself serving in H.M.S. Sea Hawk, H.M.S. Falcon or H.M.S. Heron. But then, what's in a name?

COMMANDOS AT KUWAIT

AN observer who arrived in London from Kuwait, has spoken of the successful deployment of Royal Marines Commandos in conditions where the temperature reached 124 degrees F. and incessant sand-storms reduced visibility to 200-500 yards.

In such conditions, with the thermometer never dropping below 90 degrees F. even in the early morning, the Commandos dug slit trenches when they were flown ashore from the Commando Carrier, H.M.S. Bulwark. They were the first troops to arrive; they at once secured the Kuwait airfield and water distillation plant, then proceeded to dig themselves in in a key position on a dominating ridge.

For the first few days circumstances were specially severe. A relentless

wind whipped up the sand and men suffered from sore eyes and parched lips.

No. 42 Commando (Lieutenant-Colonel E. R. Bridges) which was embarked in the carrier, was able to take up positions and hold them for 48 hours before other effective forces were available. They were put ashore, supplied and supported by the helicopters of No. 848 Squadron of the Fleet Air Arm and were fully equipped and in fighting order well before communications were established by road.

Later with bivouacs and camouflage nets providing the only shade from the gruelling sun, the helicopters of the Bulwark were welcomed as "flying angels" bringing ice, fruit and other commodities to relieve the stress of heat and to supplement food packs.

The Bulwark herself, with air conditioning, which kept the temperature below decks at an even 82 degrees, was an equable haven of comfort for men—Royal Marines and Army personnel—temporarily sent back from their arduous desert positions.

The ship also proved invaluable to No. 45 Commando (Lieutenant-Colonel L. G. Marsh, M.C.) flown in from Aden in support of 42 Commando, the following day.

42 Commando continued to fulfil its traditional role of holding ground until it was relieved in the forward position by an Army battalion. The Commando was then withdrawn into reserve to a position from which full advantage could be taken of its special helicopter-borne manoeuvrability.

45 Commando remained deployed in defensive positions until, with 42 Commando and the Bulwark, they were withdrawn from Kuwait.

In this, the first operation in which a Commando carrier has been employed, the ability of the Navy to concentrate forces speedily, unobtrusively and effectively has been amply demonstrated. It is felt that the value of the Commando ship concept has been fully confirmed.

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Falmouth Commissions

H.M.S. FALMOUTH, an anti-submarine frigate of the Rothesay class commissioned for service with the Royal Navy on July 25, at the Wallsend-on-Tyne shipyard of Swan, Hunter, and Wigham Richardson Ltd. Present at the ceremony was the Civil Lord of the Admiralty, C. Ian Orr-Ewing, Esq., O.B.E., M.P., and Mrs. Orr-Ewing, who launched the ship in December, 1959.

Frigates of this class have an armament consisting of two 4.5 inch guns in a twin mounting controlled by an electro-mechanical computer system, and a single 40 mm anti-aircraft gun which will eventually be replaced by a Seacat guided missile launcher and director. Two triple-barrelled mortars are fitted for anti-submarine warfare.

They have a length of 370 feet, beam of 41 feet and a standard displacement of about 2,000 tons. The peace-time complement is eight officers and 195 ratings.

A high standard of accommodation is incorporated in the design, including bunk sleeping, fitted bathrooms and showers and air conditioning on the mess decks. The modern galley is fitted with special exhaust ventilation, and domestic type refrigerators have been installed in all messes.

The propulsion machinery is geared steam turbine of modern and particularly light and compact design, which has proved highly satisfactory in earlier ships of the class.

The Falmouth is being adopted by the town whose name she bears and she is expected to visit the town in October.

Under the command of Cdr. R. D. Macdonald, R.N., this latest Falmouth will work up to operational efficiency at Portland and then join the 20th Frigate Squadron based at Londonderry.

Tribals Memorial Fund recalls Naval History

16 DESTROYERS WON 95 BATTLE HONOURS

New generation at sea

THIS year sees the reappearance in the Royal Navy of the famous Tribal names, with two general purpose frigates of the new Tribal class commissioning by the autumn. They are H.M.S. Ashanti and H.M.S. Gurkha, and five more of these Type 81 frigates are to follow.

The battle honours of all the wartime Tribals—of which there were 16 in the Royal Navy—are believed to be unrivalled by any other class of ship, and H.M.S. Nubian gained 13 individual battle honours between 1939 and 1945, which is a record only bettered by H.M.S. Warspite's total of 14.

To commemorate this record of service it is hoped to make a presentation to each new Tribal frigate and a committee has been formed to organise an appeal to be known as the Tribals Memorial Fund. Patrons of the fund are Admiral of the Fleet, Sir Philip L. Vian, G.C.B., K.B.E., D.S.O. and two bars, and Admiral Sir R. S. Gresham Nicholson, K.C.B., C.B., D.S.O., D.S.C. Chairman of the fund is Admiral Sir Frederick R. Parham, G.B.E., K.C.B., D.S.O.

The man who first conceived the idea of naming a flotilla of destroyers after well-known tribes chose better than he thought. His own name, unfortunately, has sunk into oblivion, but the ships he named have become a tradition in the Navy. We may, per-

haps, raise our eyebrows a little at Crusader as a tribal name—Coeur de Lion, one thinks, would not have approved—but the others were magnificent for destroyers.

Some of the first tribal names were ready-made, so to speak. Amazon and Saracen, Mohawk, Cossack and Tartar, already had a respectable history in the Navy. There had been a Tartar in 1702 which had fought at Velez Malaga with Rooke and Clowdisley Shovel, and such was her fame as a fighter that when she captured a French warship in 1705, it was added to the English fleet as H.M.S. Child's Play. In 1854, when two ships were building in British yards to the order of the Russian Navy, they were both taken over when the Crimean War was declared. Both fought against Russia in that war, and what more natural that they should be named Tartar and Cossack.

The first Tribals were ordered in 1906. They were, rather optimistically, described as ocean-going destroyers although they carried only enough oil to steam 1,500 miles

at economical speed. At full speed they did 33-34 knots, a big advance on the preceding River class with a mere 26. All the first Tribals were different, as the individual designs were left to the builders, with as much as 38 feet between the longest and shortest and 220 tons between the heaviest and lightest. Some had three funnels, others four.

THE DOVER PATROL

Throughout the First World War they served in the Dover patrol, an endless, exacting duty of long patrol and short and fierce action with few highlights. Two, the Gurkha and the Maori, were sunk by mines; two more, the Nubian and the Zulu, were badly damaged by torpedo and mine respectively. Each, however, had still a part, or half a part, to play in the Dover patrol. The undamaged fore part of the Zulu was joined to the undamaged after part of the Nubian, and the new ship sailed as the Zubian. She earned her keep when she sank the German submarine U.C.50 in 1918.

By 1919 these first Tribals had finished their careers, those that survived the war being sold for breaking up. In 1936, two new flotillas of large destroyers were ordered as a counter to the big German destroyers laid down in 1934-35 and the large Italian Scout destroyers commissioned in 1928-29. Too big and too expensive for work with the fleet, their appearance gave an opportunity to depart from the pre-war practice of giving each destroyer class names beginning with the same letter in the alphabet. The Ships' Names Committee had worked through the alphabet from "A" to "I" since the war, and here was a chance to revive names that had been well loved among destroyer men. A second generation of Tribals was born, ships whose fame was to spread all over the world.

Sixteen of the new Tribals were built, big destroyers with very long forecastles, handsome in the extreme. They carried the unusual armament for destroyers of eight 4.7-inch guns and only four torpedo tubes. All had a speed of over 36 knots, and their size, speed and armament made them formidable opponents of anything the Germans or Italians could put out in the destroyer line. About the only ocean that did not see them during the war was the Pacific, and of the 16 Tribals which served in the fleet in 1939, only four remained in 1945.

'THE NAVY'S HERE'

It was in February, 1940, that the first Tribal name sprang into the public consciousness. On the night of the 16th the Cossack, having discovered the German Altmark in Norwegian territorial waters, steamed into Josing Fiord and removed from her empty holds the British merchant seamen who had been captured by the Graf Spee during her cruise in the Indian and South Atlantic oceans. "The Navy's here," which was the cry of the Cossack's boarding party as it swarmed on board the Altmark, tickled the public fancy almost to become a naval watchword.

Hard on the Altmark incident came the second battle of Narvik on April 13. Four Tribals were engaged there, the Cossack, Bedouin, Punjabi and Eskimo, and the destruction of eight German destroyers and one U-boat without loss made it a memorable victory, though for a time it was touch and go with the Eskimo. A torpedo from an enemy destroyer blew her bows off, and for a time she was firmly anchored by the wreckage of her forecastle until the rising tide lifted her clear. She steamed down the fiord stern first to a quiet corner for running repairs, and patched herself up sufficiently for the passage home. She was back, with new bows built on to her, in five months.

She was more fortunate than the Gurkha, which sank as the result of a bombing attack off Bergen on April 9. Her loss was so deeply felt by the men after whom she was named that every officer and man in the Gurkha Brigade voluntarily subscribed one day's pay to buy a new Gurkha. The destroyer Larne, then building, was renamed to take her place, only to fall a victim to a U-boat's torpedo in the Mediterranean two years later.

BISMARCK CHASE

The chase of the Bismarck gave the Tribals another opportunity of proving their mettle. The Somali, Tartar and Mashona were escorting the Rodney and were in at the death: the Cossack, Maori, Zulu and Sikh

were even more fortunate. As the last light was dying on May 26, 1941, the four destroyers, spread on a line of bearing, found the enemy. Their job was to shadow and hold her through the night until the avenging guns of the home fleet could come up with her in the morning.

As darkness fell, the weather deteriorated, with high winds and heavy rain squalls. Although the Bismarck had been heavily damaged in an air attack from the Ark Royal, her radar-directed gunfire was uncannily accurate in the darkness. Eight 15-inch, 12 5.9-inch and 16 4.1-inch guns, expertly fired and controlled, made her still a formidable adversary for four Tribals. Yet they held her fast throughout the tempestuous night and delivered her in the morning to the King George V and the Rodney. What was more, they had in the meantime hit her twice with torpedoes.

Farther south, other Tribals were active in the Mediterranean. The Mohawk and Nubian had been at Matapan, and a fortnight later both were engaged, with two other destroyers, in an action off Sfax, in Tunisia, in which three Italian destroyers and a convoy of five supply ships were obliterated. Most of the big Mediterranean actions—the convoys to Malta, the second battle of Sirte, Greece and Crete—had their Tribal representatives, and the number of smaller actions up and down those waters in which they fought was legion.

ONLY FOUR LEFT

By 1944, the tide of war, which had turned earlier and was now flowing strongly towards an allied victory, no more than four Tribals were left out of the original 16, the Ashanti, Eskimo, Nubian and Tartar. All four were engaged in the Normandy landings, and with that great operation safely launched, the Tartar, Nubian and Eskimo sailed for the Indian Ocean to take a part in the war against Japan.

Between them, the 16 Tribals collected no fewer than 95 battle honours, of which the Nubian was responsible for 13 and the Tartar for 12. In the whole fleet, only the ubiquitous Warspite put up a nobler score with 14, and only one other destroyer, the Jervis, equalled the Nubian's bag.

The bones of these famous ships lie scattered up and down the oceans of the world. Three of them reached their end in the Arctic, one of them tragically under the bows of King George V in a thick fog. Two lie off Tobruk, victims of the fierce fight for that bleak fortress on the North African shore. Two more, one of them the Cossack, mingle their bones with those of that great host of ships which met their end in the Atlantic.

When the war ended in 1945, the day of these Tribals was over. They had made their names known and admired throughout the Naval world, catching the eye and the ear of the public as, perhaps, no other destroyers had done, and adding a second "cachet" to the older Tribals of the Dover patrol in the first German war. The four survivors of 1945 did not last long. Naval technology was advancing too fast for them and the new weapons being evolved overtook their usefulness. By 1949, all were gone.

THE THIRD GENERATION

A third generation of Tribals is on the stocks, and the first of them, the Ashanti, should be at sea this year. These new Tribals are general purpose frigates, the first frigates in the fleet designed to carry a helicopter for anti-submarine reconnaissance. Seven of them have been ordered, and their names ring like bells with the memories of their illustrious ancestors. Ashanti, Eskimo, Gurkha, Mohawk, Nubian, Tartar and Zulu; in the Navy, these are names to conjure with.

The Tribals Memorial Fund will, through its presentations to the new generation of ships, help to link the old names with the new. Treasurer of the fund is Capt. E. N. Sinclair, D.S.C., R.N., Captain of H.M.S. Sea Eagle, Londonderry, Northern Ireland, who is hoping to receive contributions from not only all those officers and ratings of the Royal Navy who served in the old Tribals, but also those of the Commonwealth navies who fought in this famous class from 1938 onwards.

Contributions, which in the case of individuals it has been suggested should be limited to two guineas, should be made out to the Tribals Memorial Fund and forwarded to Capt. Sinclair at H.M.S. Sea Eagle.

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H.M.S. Puma's children's party at Gdynia on July 5

Puma leaps the Iron Curtain

POLISH TRIP NONE WOULD HAVE MISSED

(BY OUR SPECIAL CORRESPONDENT)

AFTER two weeks or so of the N.A.T.O. Exercise "Fairwind," which included visits to Invergordon and Aarhus, we arrived in Copenhagen on June 26, in company with H.M.S. Bermuda and H.M.S. Undaunted. Copenhagen is almost the sailors' paradise, but this ship's company has been spoilt by the flesh pots of South America. However, with free trips to the Carlsberg and Tuborg breweries, an excellent dance and night spots open until five in the morning a fair percentage of us managed to spend a lot of money and have a good time as well.

Puma, on leaving Copenhagen in company with H.M.S. Blackwood, sailed south to the Baltic Sea and on July 3 steamed into the Polish port of Gdynia, where, for the majority of us, we had our first glimpse behind the Iron Curtain. As the ship berthed alongside we were welcomed by a large naval guard and band and a considerable number of civilians. After the arrival ceremony the Polish Naval Commander-in-Chief visited the ship. A reception for over a hundred took place in the Naval Barracks during

the first afternoon and a very good lunch, which included much beer and vodka, was laid on. Although the language difficulty was extreme, by the end of the reception everyone was carrying out a conversation of some kind, mostly by hand signals. The Poles had a very nice habit of giving their guests a leaving present and we all received either an ashtray, an album or a hand-carved box. The Customs showed keen interest in these on our eventual arrival in the U.K.

VISIT TO WARSAW

Next day the Captain, accompanied by twenty officers and chief Petty Officers, went by air to Warsaw.

Warsaw is a city that has been completely rebuilt, since very little of it was left standing. There are very few private houses and blocks of flats are the only answer to their housing problem. They are all identical—like rows of boxes—and none of the buildings are finished off—no plaster nor paint and how drab they all looked! Even paths and pavements are a luxury as there are so few of them.

A stop was made at the Tomb of Poland's unknown soldier, where the captain laid a wreath. Then on to the British Embassy where the Consul and staff made us welcome. The food was delicious after a hot and dusty tour.

GIGANTIC LUNCH

For lunch we were guests of a Polish general who started off with a speech in Polish but his interpreter made such a hash of it that the general had to wave him down. From then on our host spoke excellent English! The lunch was a gigantic affair which lasted well into the afternoon and very few of the party were awake on the return flight to Gdynia. All agreed it was a very instructive and enjoyable trip.

A most successful children's party was held during the afternoon of our last day for fifty orphans. The children, some in their National costume, were very smartly dressed and they insisted on entertaining us with music and dancing. Little tappers of about seven were strapping on piano accordions and rattling out tunes quite professionally. We all had a smashing time!

Our guard landed twice for wreath laying ceremonies and they seemed to impress the Poles by their smart appearance and bearing. Quite a few coach tours were laid on for sight-seeing and transport was provided for bathing parties. The ship was open to visitors and some 1,500 came aboard in the three hours—a record for us.

We sailed from Gdynia on July 6. I doubt if anyone would willingly have missed the trip to Poland, but to sum it up in the words of an Able Seaman who, when asked what he thought of the Polish way of life, said "I am quite happy being a bloated capitalist."

We complete our commission on August 22, arriving at Devonport on August 8 via Gourock, Londonderry and Portsmouth. I am sure that in years to come all Puma's Ship's Company will talk about the wonderful times we have had in the last two years. Twenty-six countries, 71,000 miles, a lot of work and a lot of fun.

NEPTUNE'S SCRAPBOOK



Admiral Sir Deric Holland Martin has relieved Admiral Sir Alexander Bingley as Commander-in-Chief, Mediterranean and Commander-in-Chief, Allied Forces Mediterranean. Admiral and Lady Bingley left Malta in H.M.S. Surprise on June 27 and Admiral Holland Martin took over command in Genoa on June 30. When Admiral Bingley left the Customs House, Malta, to embark on Surprise on June 27 he was pulled off to the ship in a whaler manned by Vice-Admiral H. C. D. Maclean, Rear-Admiral D. H. F. Hetherington, Surgeon Rear-Admiral W. V. Beach, Rear-Admiral J. C. C. Henley and Rear-Admiral J. F. D. Bush.

Rear-Admiral Michael Le Fanu, D.S.C., has been relieved as Flag Officer Second-in-Command, Far East Station, by Rear-Admiral J. B. Frewen. Rear-Admiral Le Fanu arrived in Singapore after conducting Fleet visits to Hong Kong and Japan. Before leaving Singapore he made a ceremonial passage through Keppel Harbour, flying his flag in H.M.S. Belfast (Capt. M. C. Giles, D.S.O., O.B.E., G.M., R.N.) and accompanied by H.M.S. Blackpool, (Commander M. L. Stacey, R.N.).

Capt. H. R. Law, O.B.E., D.S.C., R.N., was promoted to Rear-Admiral, to date July 7.

Capt. P. D. Gick, O.B.E., D.S.C. and Bar, R.N., was promoted to Rear-Admiral, to date July 7.

Rear-Admiral D. H. F. Hetherington, C.B., D.S.C. and Two Bars, was placed on the Retired List to date July 15.

Capt. R. A. J. Owen, R.N., was promoted to Rear-Admiral, to date July 7.

Surg. Capt. A. D. Petro, V.R.D., M.R.C.S., L.R.C.P., R.N.R., has been appointed an Honorary Physician to The Queen, in succession to Surg. Capt. P. de B. Turtle, V.R.D., M.A., B.Ch., M.R.C.S., L.R.C.P., D.P.H., R.N.R.

Vice-Admiral Sir Royston Wright, K.C.B., D.S.C. and Bar, has been appointed a Lord-Commissioner of the Admiralty, Second Sea Lord and Chief of Naval Personnel in succession to Vice-Admiral Sir John Tyrwhitt, Bt., K.C.B., D.S.O., D.S.C. and Bar, the appointment to take effect in September.

Rear-Admiral A. R. Hezlet, C.B., D.S.O. and Bar, D.S.C., has been appointed Flag Officer, Scotland, in succession to Vice-Admiral Sir Royston Wright, K.C.B., D.S.C. and Bar, the appointment to take effect in September.

Rear-Admiral P. D. Gick, O.B.E., D.S.C. and Bar, is to be Flag Officer, Flying Training, in succession to Rear-Admiral F. H. E. Hopkins, C.B., D.S.O., D.S.C., the appointment to take effect in October.

Vice-Admiral P. W. Gretton, C.B., D.S.O. and Two Bars, O.B.E., D.S.C., is to be a Lord-Commissioner of the Admiralty, Deputy Chief of Naval Staff and Fifth Sea Lord in succession to Admiral Sir Laurence Durlacher, K.C.B., O.B.E., D.S.C., the appointment to take effect in January, 1962.

Rear-Admiral H. R. Law, O.B.E., D.S.C., is to be Flag Officer Sea Training in succession to Vice-Admiral P. W. Gretton, C.B., D.S.O. and Two Bars, O.B.E., D.S.C., the appointment to take effect in December.

Rear-Admiral M. S. Townsend, C.B., D.S.O., O.B.E., D.S.C. and Bar, was placed on the Retired List, to date July 7.

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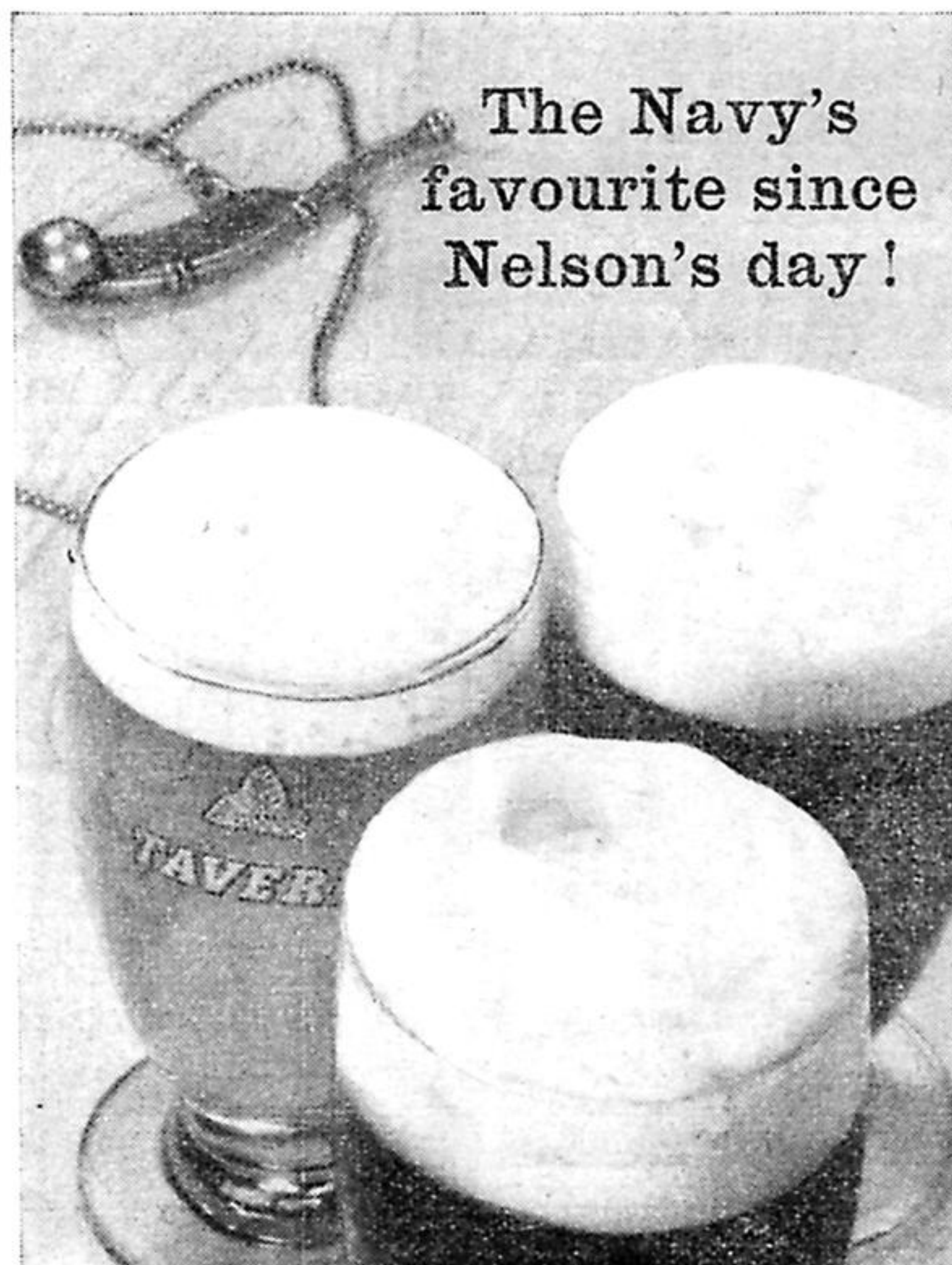
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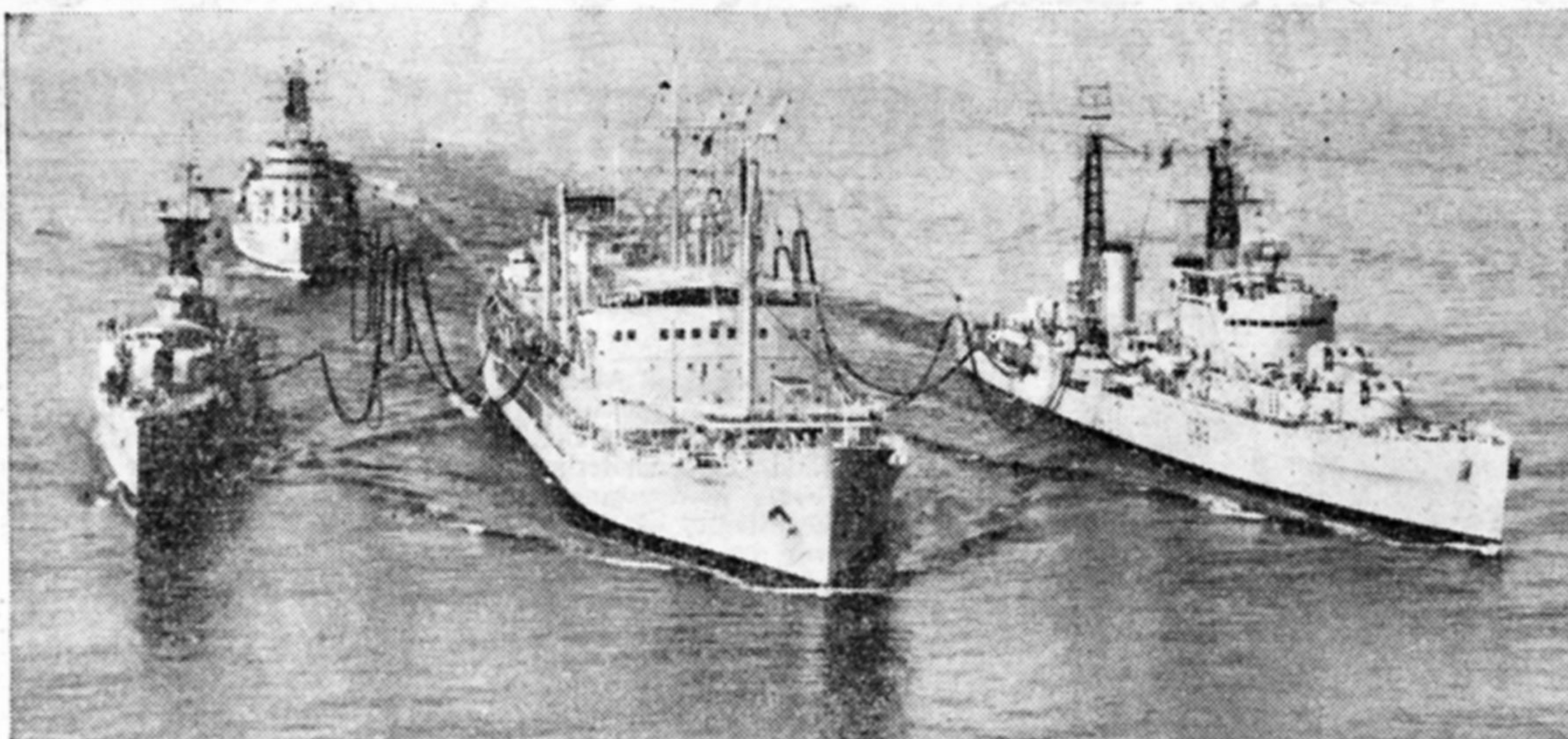
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The Fast Fleet Replenishment Ship Tidereach refuelling three ships simultaneously, the frigate H.M.S. Yarmouth (left), the New Zealand cruiser Royalist (right) and, in rear, H.M.S. Belfast. Designed for the support of the fleet and the replenishment of its supplies under way, Tidereach was built by Swan, Hunter and Wigham Richardson, Ltd., Wallsend-on-Tyne and completed in 1955



A Westland Wessex helicopter lifts a Citroen car from the flight deck of H.M.S. Bulwark

Vice-Admiral Sir Charles Madden, Bart., C.B., inspecting ship's company of H.M.S. Sea Eagle, Londonderry, during his recent visit to Northern Ireland.

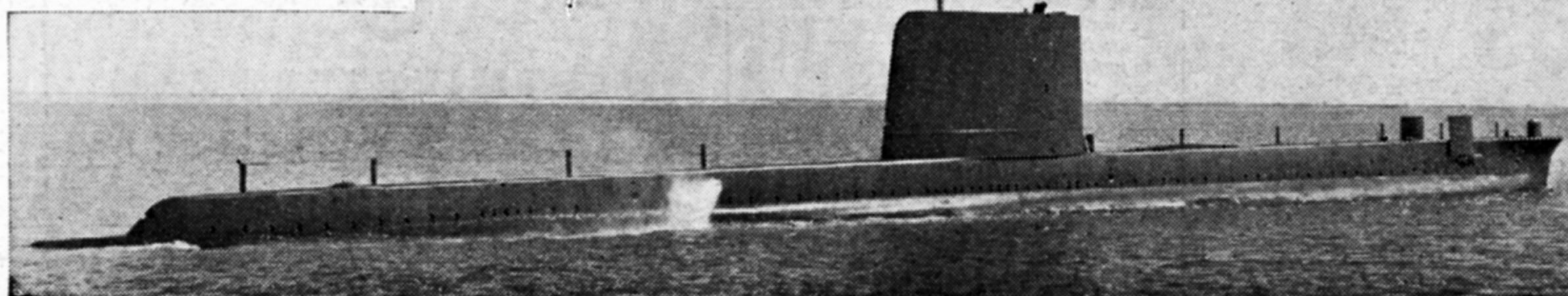


One of the highlights of the recent Royal Tournament was the display by Royal Naval Juniors of cutlass drill. Their "hornpipe" was greeted with applause night after night. In the centre is one who really needs no introduction—Mr. Harry Secombe—who thoroughly enjoyed the juniors' show.



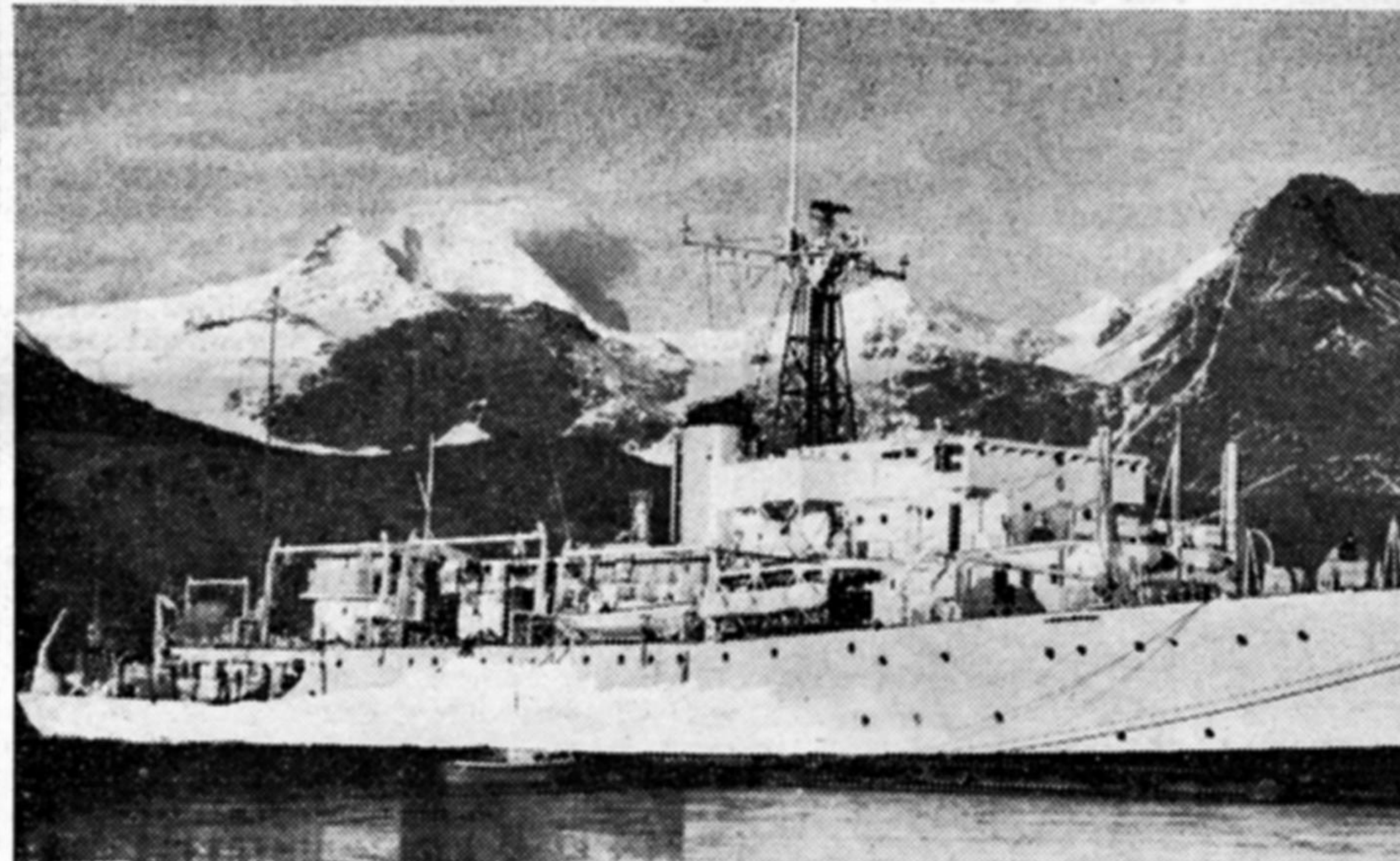
H.M.S. Finwhale, a Porpoise class submarine built by Vickers-Armstrongs Ltd.

H.M. Submarine Porpoise, the first of its class, completed in 1958 to undertake continuous submerged patrols in any part of the world

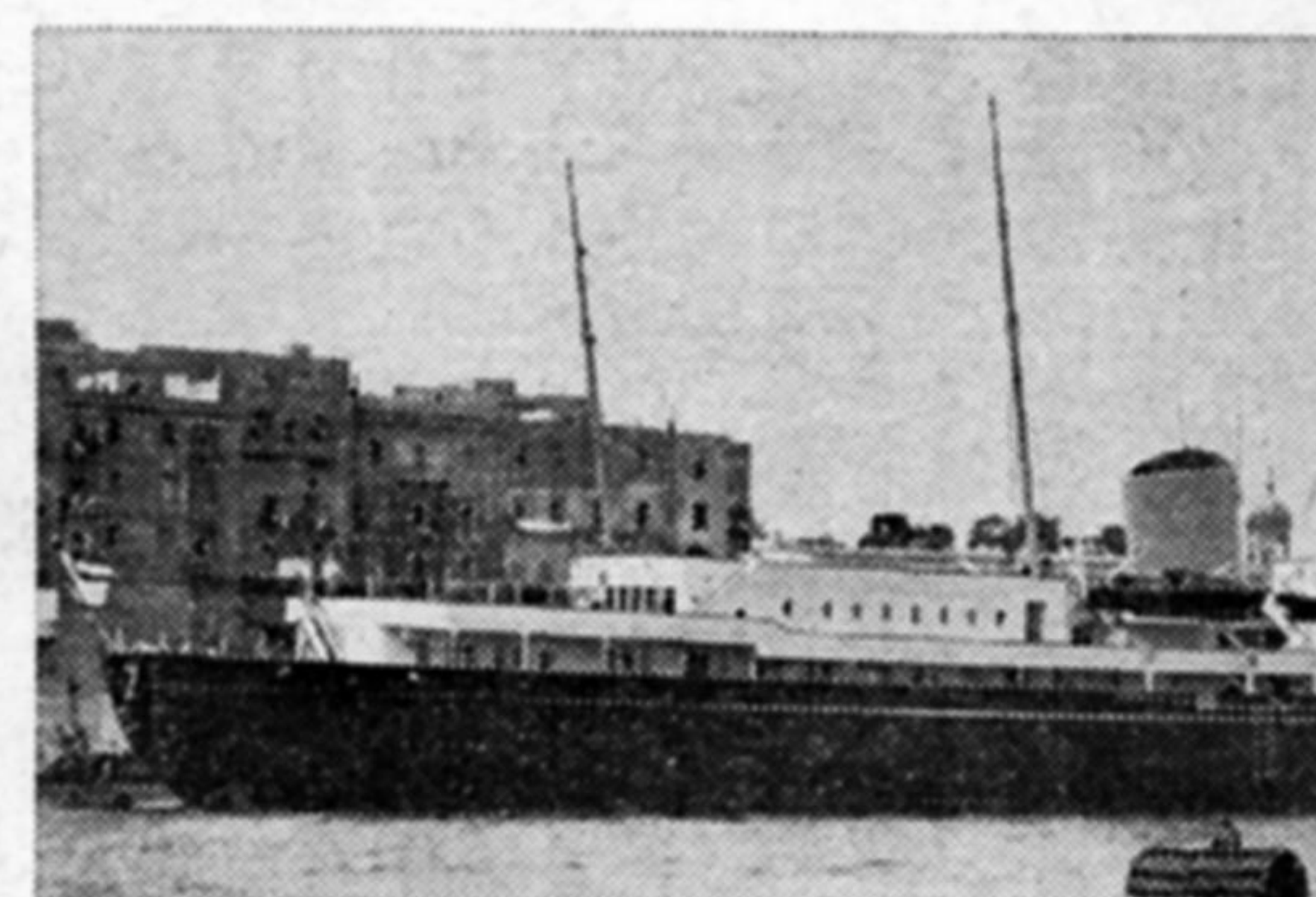


H.M.S. Lion—one of the last conventional cruisers now in the Mediterranean

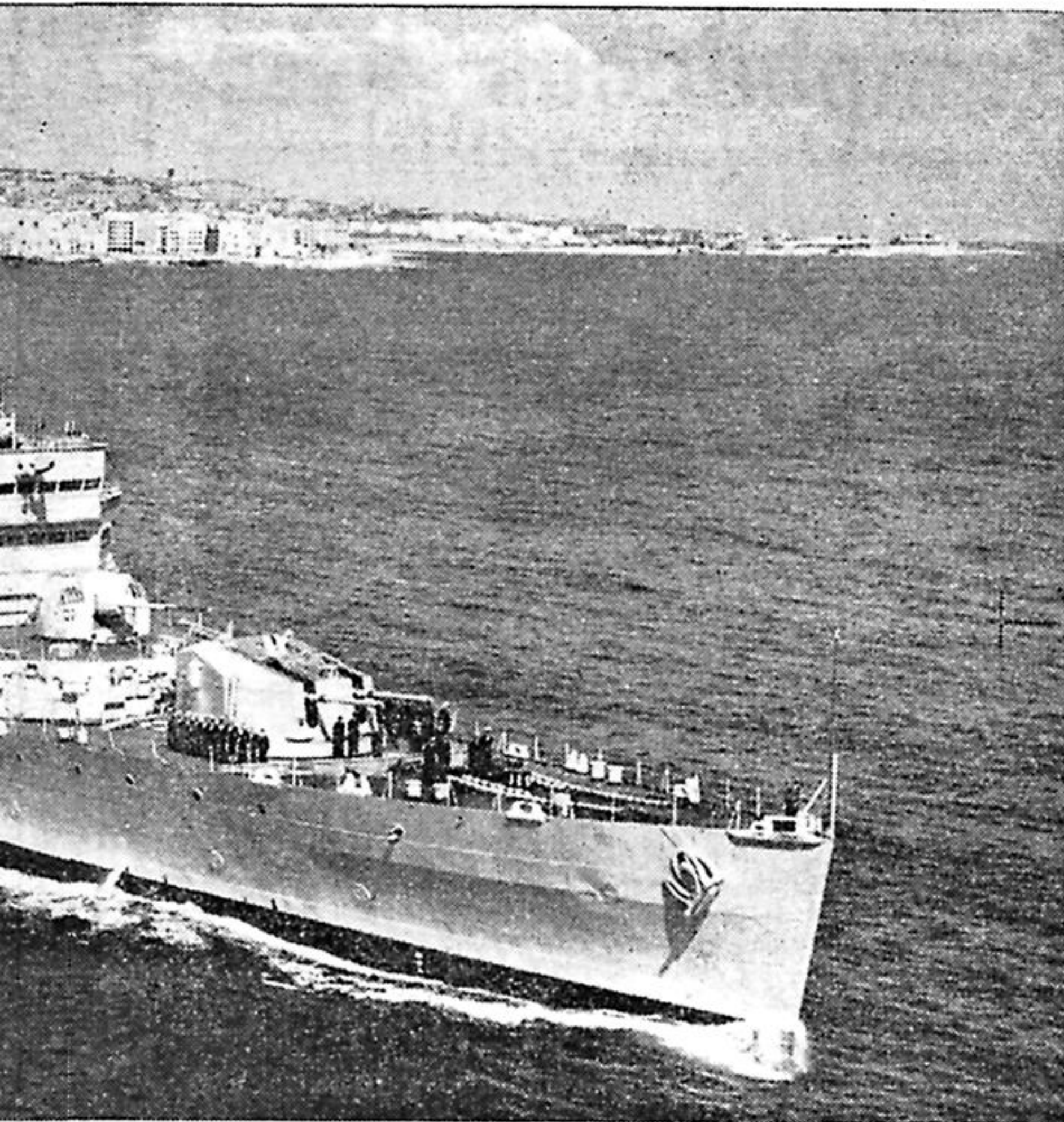
MANY PHASES OF



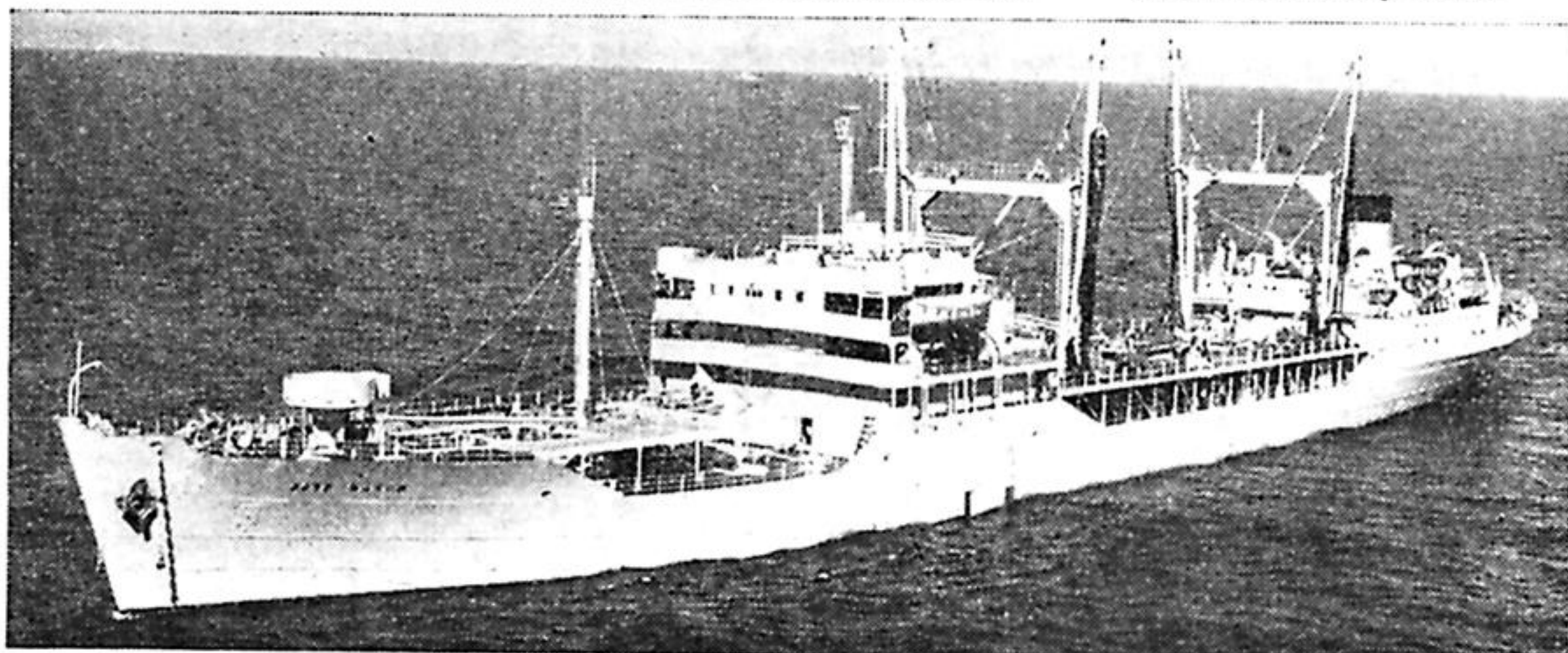
H.M.S. Owen (Cdr. G. P. D. Hall, D.S.C., R.N.), the ship, against the rugged background of South Georgia Island, Edward Cove



H.M.S. Striker is a Landing-Ship Tank (As)



The coastal minesweeper H.M.S. Shavington in Malta with the Turkish ship Kilicali Pasha (middle) and the Italian ship Driade

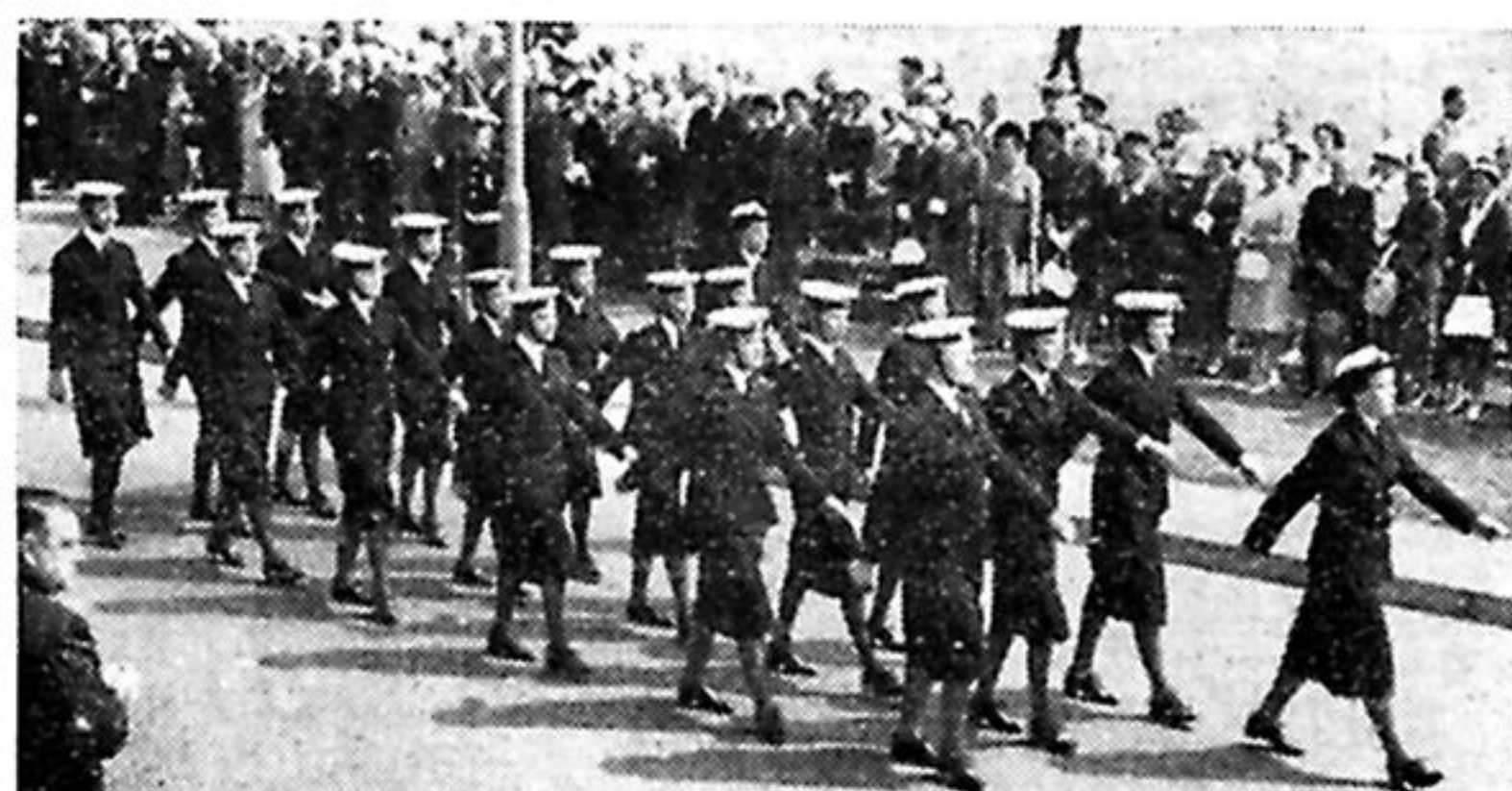


The oiler R.F.A. Wave Baron was launched in 1946 and was built by the Furness Shipbuilding Co. Ltd., Haverton Hill—on Tees. Her displacement (full load) is 16,480 tons

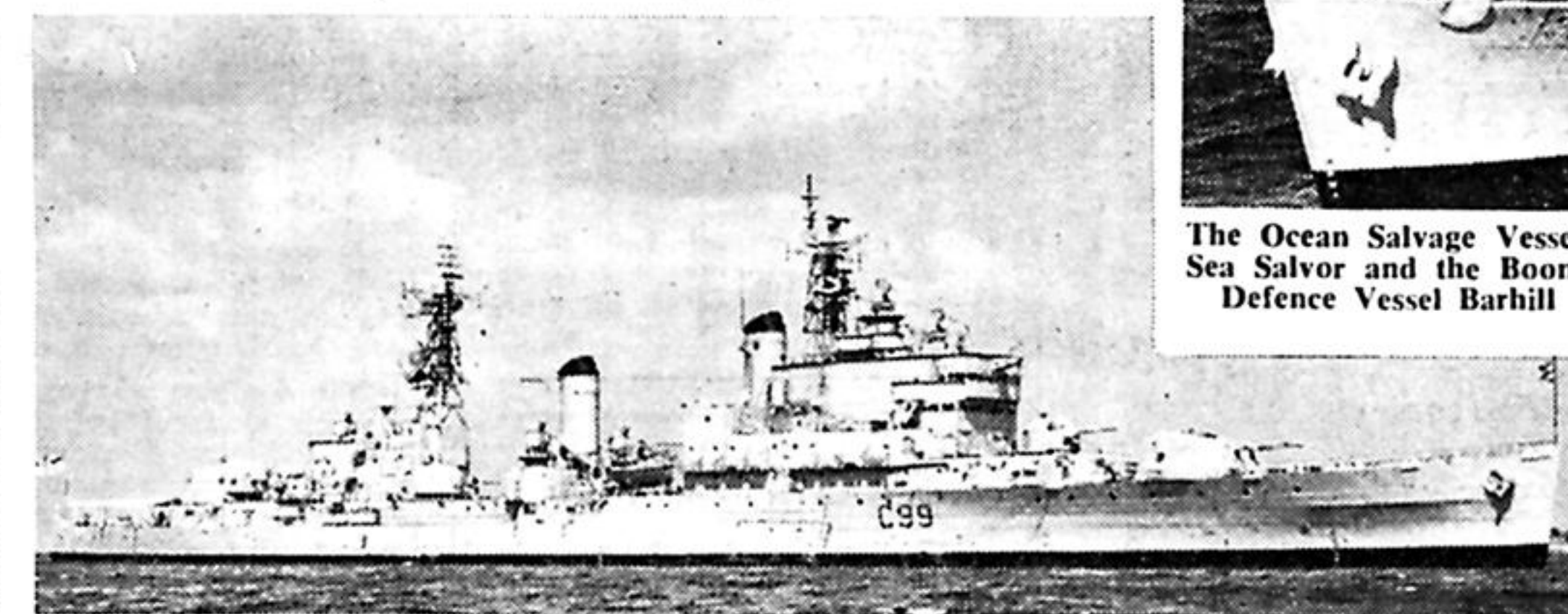
THE ROYAL NAVY



Royal Naval Surveying ship, at anchor in King's Bay



The W.R.N.S. Unit of the Depot, Royal Marines, Deal, showing a smartness in marching which must have come from their male counterparts whose precision is so much admired



H.M.S. Blake, the last of the three Tiger class cruisers, is of 11,700 tons displacement (full load). Accommodation is of a very much higher standard than in any previous cruisers

H.M. Yacht Britannia returned from a Mediterranean cruise in June



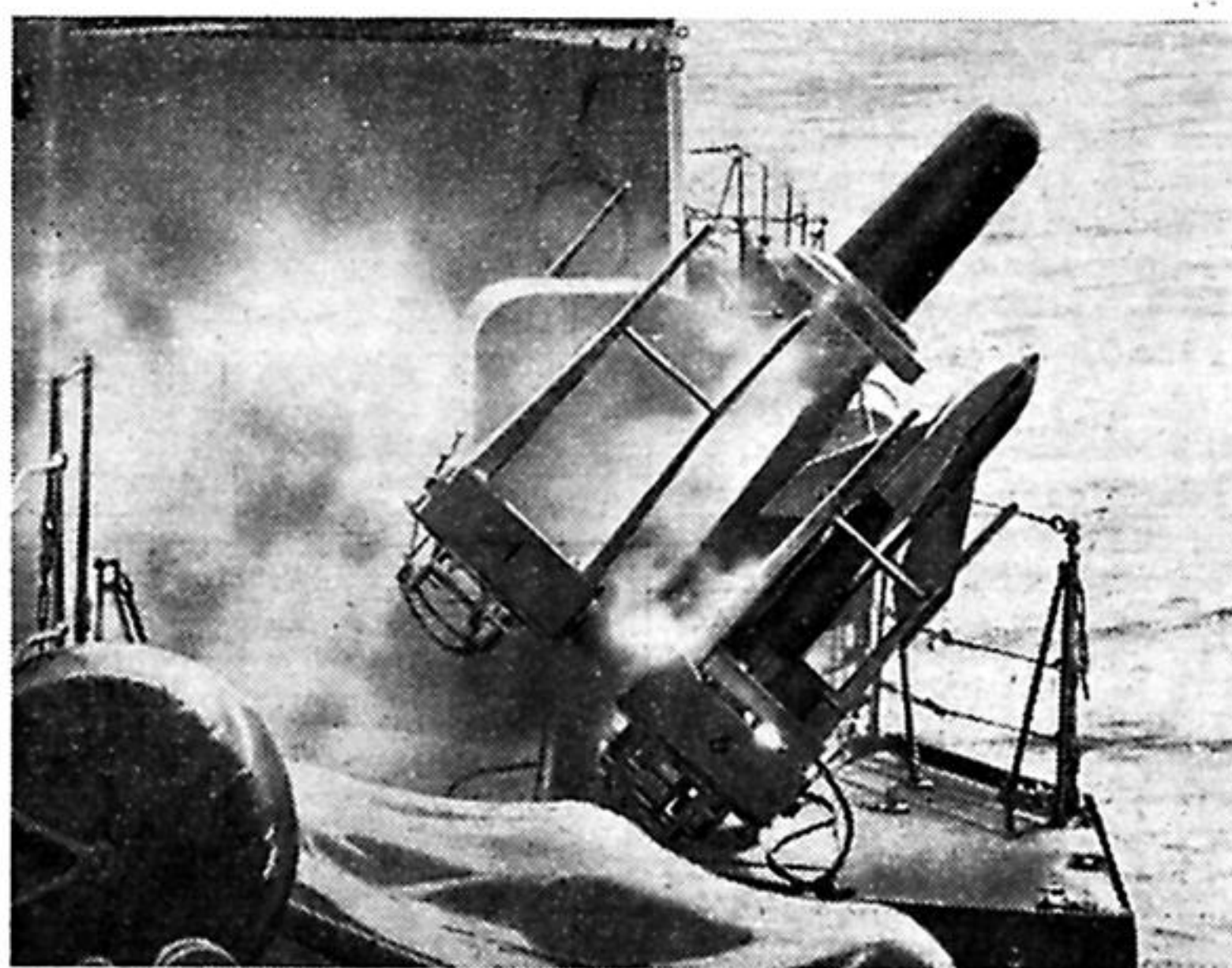
(assault) of 5,000 tons displacement (full load)



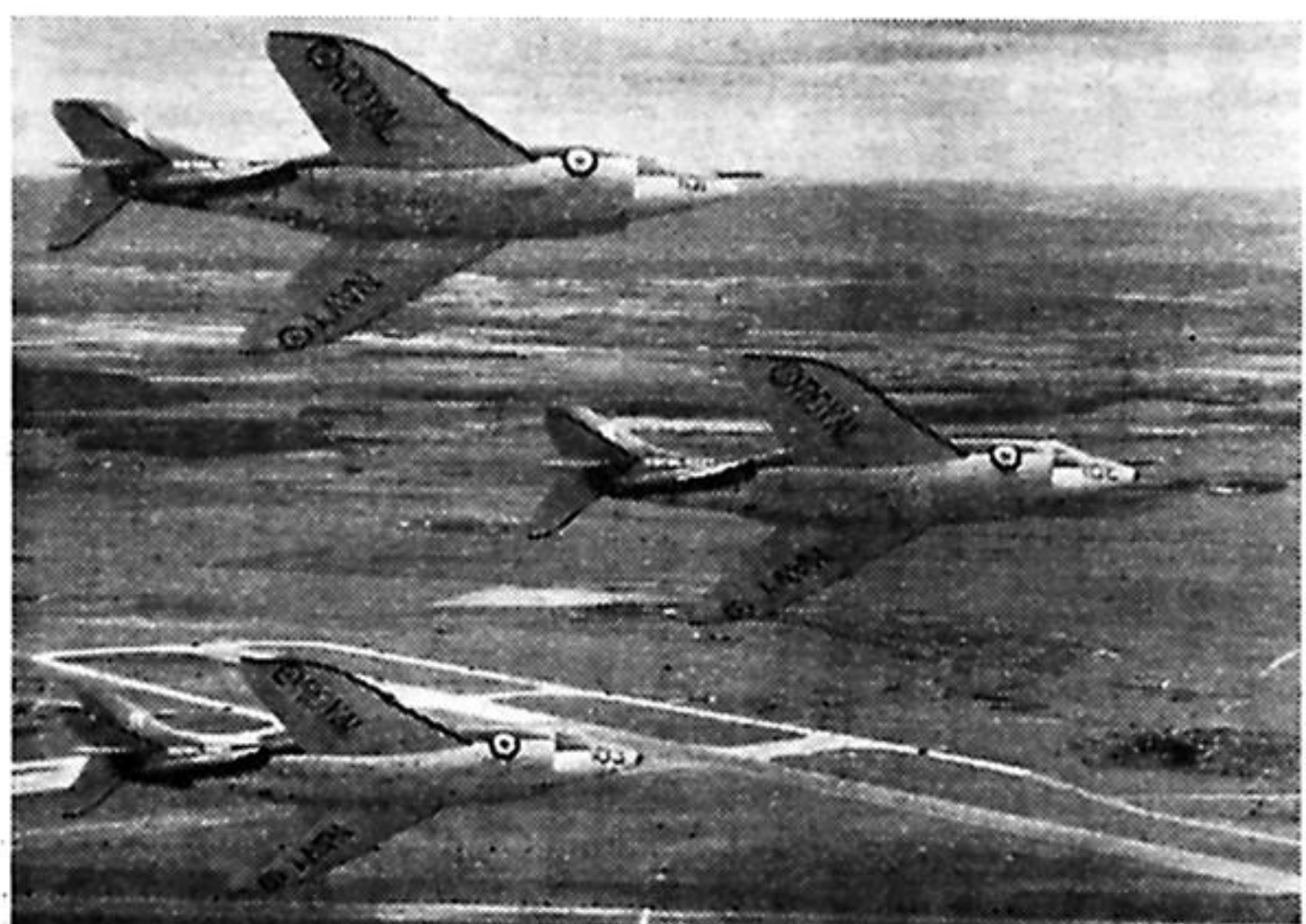
The Ocean Salvage Vessel Sea Salvor and the Boom Defence Vessel Barhill



The M.Y. Mercury, built by Vosper Ltd., for Mr. Stavros Niarchos. Believed to be the fastest private yacht in the world



Extensive and successful trials have been taking place in H.M.S. Decoy with the Seacat surface-to-air guided missile. Here is a Seacat on its launching ramp



Scimitars of 800 Naval Air Squadron which took part in the Paris Air Show and are to take part, too, in the Farnborough Air Show

College is used by U.S. as amphibious base

CHANGES IN ENTRY

In the July issue of "Navy News" the transition of the training of young men to become officers in the Royal Navy from the Naval Academy at Portsmouth (1733 to 1837) to the training of cadets at sea at Portsmouth, Portland and eventually at Dartmouth, and the need for a college which resulted in the building of the Britannia Royal Naval College at Dartmouth (1902 to 1905) was dealt with. Last month's issue took the story to 1940.

IN 1942 the college was bombed. Luckily few people were present at the time and the casualties were one Wren killed and an officer wounded. Evacuation had to be effected, however, and Mullers Orphanage at Bristol was converted to take the seven junior terms, the senior terms, of which there were four, remaining at Dartmouth.

The college was reunited at Eaton Hall, the Duke of Westminster's Cheshire seat, in February, 1943, remaining there until September, 1946.

OCCUPIED BY U.S. NAVY

From the time of evacuation of cadets from Dartmouth, the college was used by our Combined Operations forces for training. Then, from December, 1943 to February, 1945, it was occupied by the United States Navy, as the United States Advanced Amphibious Base for Combined Operations.

Prior to D Day the college grounds were used for parking tanks, lorries, mobile artillery and weapons that form part of an invading force, which the Americans had stationed in the Dartmouth area.

Officers and men of the United States Navy packed the college chapel on Sunday, April 29, 1945, for a service and presentation of a stained glass window by Commodore Korns, Commander of the Amphibious Base, United Kingdom, on behalf of the men of the United States Navy.

He said the personnel of the United States Navy had constantly striven to preserve the college and its traditions, and as the ships and men had left the port on time and in numbers as originally planned they had somewhat achieved this.

Capt. Warner accepted the gift on behalf of the college and stated that it would be appreciated throughout the Service, and after their return to Dartmouth would help them to recall the important work of sea power, which had enabled the Allied Armies to invade France.

Much repair was needed at the college, and the end of the war gave an opportunity for alterations to be carried out. The library was doubled in its size, the science department was

given extra space, a separate block was begun to provide studies for the evicted officers and masters; and automatic telephone exchange system, loud hailer and fluorescent lighting were installed.

Capt. P. B. R. W. William-Powlett, C.B.E., D.S.O., who had been one of the cadets sent to sea in 1914, without completing their course, was appointed to command the college in January, 1946.

Christmas term, 1946, Vice-Admiral Aubrey Smith, U.S. Navy, spent a week-end with the Captain. After Divisions on Trafalgar Day, when addressing the cadets, he drew their attention to the lessons that could be learned by studying Nelson's life and methods. He said the United States still considered such a study as an aid to efficiency.

During the winter, 1947, Viscount Mountbatten of Burma lectured on the South-East Asia Campaign. At the end of his lecture he presented the college with a Japanese sword. (This can be seen in a glass case outside the Quarterdeck.)

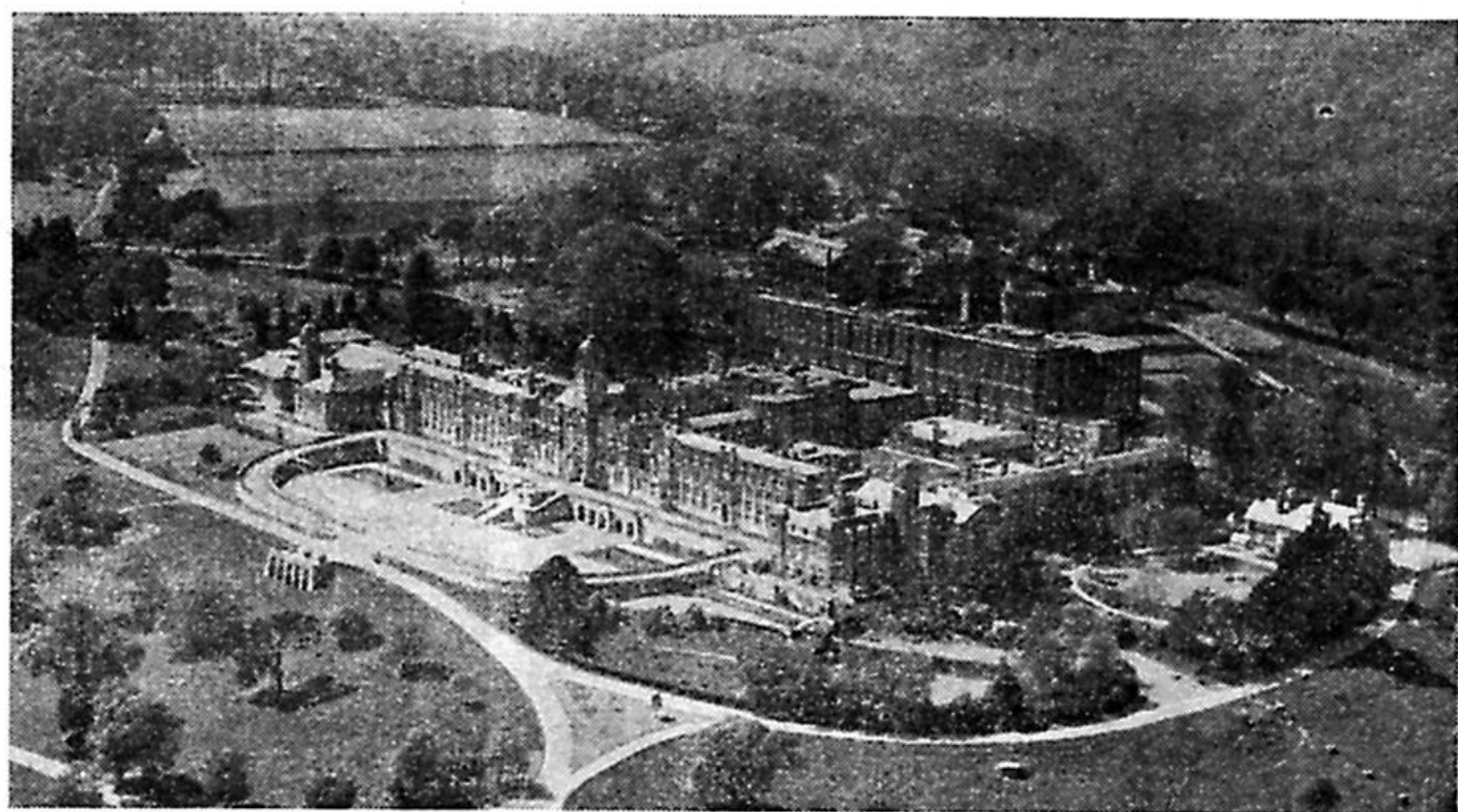
The statue of King George V had been chipped by the bombing of the college. King George VI, during a visit in 1947, asked that it should not be restored, but remain as a memorial.

CHANGES

More changes were contemplated and some introduced, the major one being that after September, 1948, cadets would join Dartmouth between the ages of 16 and 16 and four months for a two-year course. They would be eligible for executive, engineering and supply branches, and the future policy would be that officers promoted from the lower deck would form 20 to 25 per cent of the whole, the remainder being drawn equally from the Dartmouth system and special entry.

When these changes had been announced in the House of Commons some eight months before, Mr. Churchill had protested vigorously against the two-year cut in the Dartmouth course.

"Truth" published an article, part of which was: "Just as the R.N. was the framework of the British war-time fleet, so was Dartmouth the frame-



For nearly 60 years Britannia Royal Naval College, Dartmouth, has been training officers for the Fleet. On the river Dart, 'mid the rolling countryside of South Devon, it has served the Royal Navy well

work on which was built the greatness of the R.N. Since the opening of the college in 1905 it has provided a steady stream of young officers, who have gloriously repaid the work of their mentors. Six thousand seven hundred cadets had passed through the college. During the late war some 550 of them gave their lives for their country." (This is now known to be 593.)

The new scheme of 1948 did not take effect immediately, but was grafted with the old, and would therefore take four and a half years to change from the old to the new. This was sensible and it did eliminate most of the problems that the Selborne scheme had met.

'ABSORB TRADITION'

A bone and ivory model of the Britannia 1820, believed to be the work of French prisoners of war who had chosen not to be repatriated after the Napoleonic Wars, was exhibited in London in 1949. Naval officers over-subscribed to buy this as a present for the college. On Sunday, October 9, after inspecting the cadets at divisions and taking the salute at the march past, Admiral of the Fleet The Earl of Cork and Orrery, presented it to the college. In his address afterwards, he urged the cadets to "absorb

tradition": "Absorb it so that it finds its way into the subconscious mind. Absorb it so that in a jam your reactions are automatic."

Anyone who has served with the admiral will know that it has stood him in good stead for many years and on many occasions.

On October 29, 1949, Admiral of the Fleet Lord Cunningham of Hyndhope unveiled and Dr. W. L. Anderson, Bishop of Salisbury and a former assistant chaplain of the college, dedicated the shrine, which had been constructed in the entrance lobby to the chapel. Within the shrine is a glass-topped case containing the Book of Remembrance of the First World War with its 332 names and that of the Second World War with its 593.

A light shines on the books night and day.

MORE CHANGES

Since 1950 the college has seen many changes and various schemes. With the end of the old 13-year-old scheme cadets were able to enter the college at 16 or 18. It was decided later to change the system completely and train cadets at the college from the age of 18 with sufficient professional knowledge that they no longer had to do the sub-lieutenant's courses at the various schools.

As a result cadets entered the college at 18 or 19 and did seven terms based at Dartmouth, two of which were to be spent in the Dartmouth Training Squadron.

This squadron was formed in 1955, but did not commence training until 1956. It now consists of H.M.S. Roebuck, Venus, Wizard and Urchin. These ships represent the first division. Three of these ships go on cruises during term time to take the place of

the training cruiser, while one remains in dockyard to refit.

The second division of the Dartmouth Squadron was formed in 1955, and was in operation with H.M.S. Jewel only in early 1956, being joined by H.M.S. Acute at a later date. These two ships still form the second division. They take the early-term cadets to sea and in harbour for practical seamanship, navigation, engineering, seaboard drill and the senior terms for practical minesweeping.

ONE PLACE TO TRAIN

In September, 1960, the scheme of training was once again altered on the recommendations of the Murray Committee. This involves an annual entry in September each year followed by two terms as a cadet at the Britannia Royal Naval College and one term in the Dartmouth Training Squadron. Cadets are then promoted midshipmen and serve for one year with the fleet. Then the executive and supply officers will return to Dartmouth for a further two years, one academic and one professional as acting sub-lieutenants, while engineer and electrical officers will continue their technical training at R.N.E.C. Manadon and at Cambridge.

The reason for this latest change was that there is only one place to train midshipmen and that is at sea. The Murray Scheme is in effect broadly similar to the traditional method of training young officers for the Royal Navy.

Upper yardmen are trained at the college and supplementary list cadets also do their initial training here. These consist of the supplementary list seamen, fixed wing and helicopter pilots and observer officers who have joined the Navy for a certain period.

The teaching of young officers may change from time to time, but the tradition of Dartmouth and its young Nelsons will never change. Admiral of the Fleet Lord Chatfield paid tribute with these words: "If the Naval officer ever fails, it will not be the fault of Dartmouth."

LIST OF COMMANDING OFFICERS

Capt. W. E. Goodenough, 1905; Capt. T. D. W. Napier, 1907; Capt. H. Evan-Thomas, M.V.O., 1910; Capt. The Hon. Victor A. Stanley, M.V.O., 1912; Capt. T. D. W. Napier, M.V.O., 1914; Capt. E. Hyde-Parker, 1914; Capt. N. C. Palmer, C.V.O., 1915; Capt. W. G. E. Ruck Keene, M.V.O., 1916; Capt. E. La T. Leatham, C.B., 1919; Capt. F. A. Marten, C.M.G., C.V.O., 1921; Capt. The Hon. H. Meade, C.B., C.V.O., D.S.O., A.D.C., 1923; Capt. M. E. Dunbar-Nasmith, V.C., C.B., 1926; Capt. S. J. Meyrick, 1929; Capt. N. A. Wodehouse, 1931; Capt. R. V. Holt, D.S.O., M.V.O., 1934; Capt. F. H. G. Dalrymple-Hamilton, 1936; Capt. R. L. B. Cunliffe, 1939; Capt. E. A. Aylmer, D.S.C., 1942; Capt. G. H. Warner, D.S.C., 1943; Capt. P. B. R. W. William-Powlett, C.B.E., D.S.O., 1946; Capt. H. W. Faulkner, C.B.E., D.S.O., 1948; Capt. N. V. Dickinson, D.S.O. and Bar, D.S.C., 1949; Capt. R. T. White, D.S.O., 1951; Capt. W. G. Crawford, D.S.C., 1953; Capt. W. J. Munn, D.S.O., O.B.E., 1956; Capt. F. H. E. Hopkins, D.S.O., D.S.C., 1958; Capt. H. R. Law, O.B.E., D.S.C., in command.

In Memoriam

Eric Kenyon, 41st Commando, Royal Marines, Marine, RM.20148. Died May 14, 1961.
Michael James Summerfield, Able Seaman, P/J.972181, H.M.S. Tyne. Died June 10, 1961.
Archibald Laurie McLean, Chief Air Fitter A/E, L/FX.76240, H.M.S. Sanderling. Died June 12, 1961.
David Frederick Shears, Able Seaman, D/J.973702, H.M.S. Drake. Died June 14, 1961.
Ian Francis Grant, Able Seaman, P/J.950778, H.M.S. Anchorite. Died June 19, 1961.
Bernard Alan Almond, Bugler, RM.16005, 41st Commando, Royal Marines. Died June 25, 1961.
Peter John Arnold Lambert, Leading Radio Operator (S), P/J.947538, H.M.S. Victory. Died June 27, 1961.
John Samuel Ramson, Chief Mechanician (E), D/KZ.84460, H.M.S. Orion. Died July 1, 1961.
Ralph Smith, Marine, RMV. 203718, 40th Commando, Royal Marines. Died July 1, 1961.
Frank Grant Halls, Radio Electrical Artificer 1st Class, L/FX. 669392, H.M.S. Ariel. Died July 6, 1961.
Anthony William George Slaught-ter, Leading Air Mechanic (O), L/FX.910682, H.M.S. Seahawk. Died June 24, 1961.



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That's what I thought when I was your age until someone showed me the Progressive Savings Scheme. I only had to put aside £3 a month by Naval allotment but when I leave the Service next year I can collect £855.

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wife would have received the whole £855 immediately. You see, it's a Savings Scheme and Life Insurance rolled into one.

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* For members of the W.R.N.S. the Pension is £149 a year.



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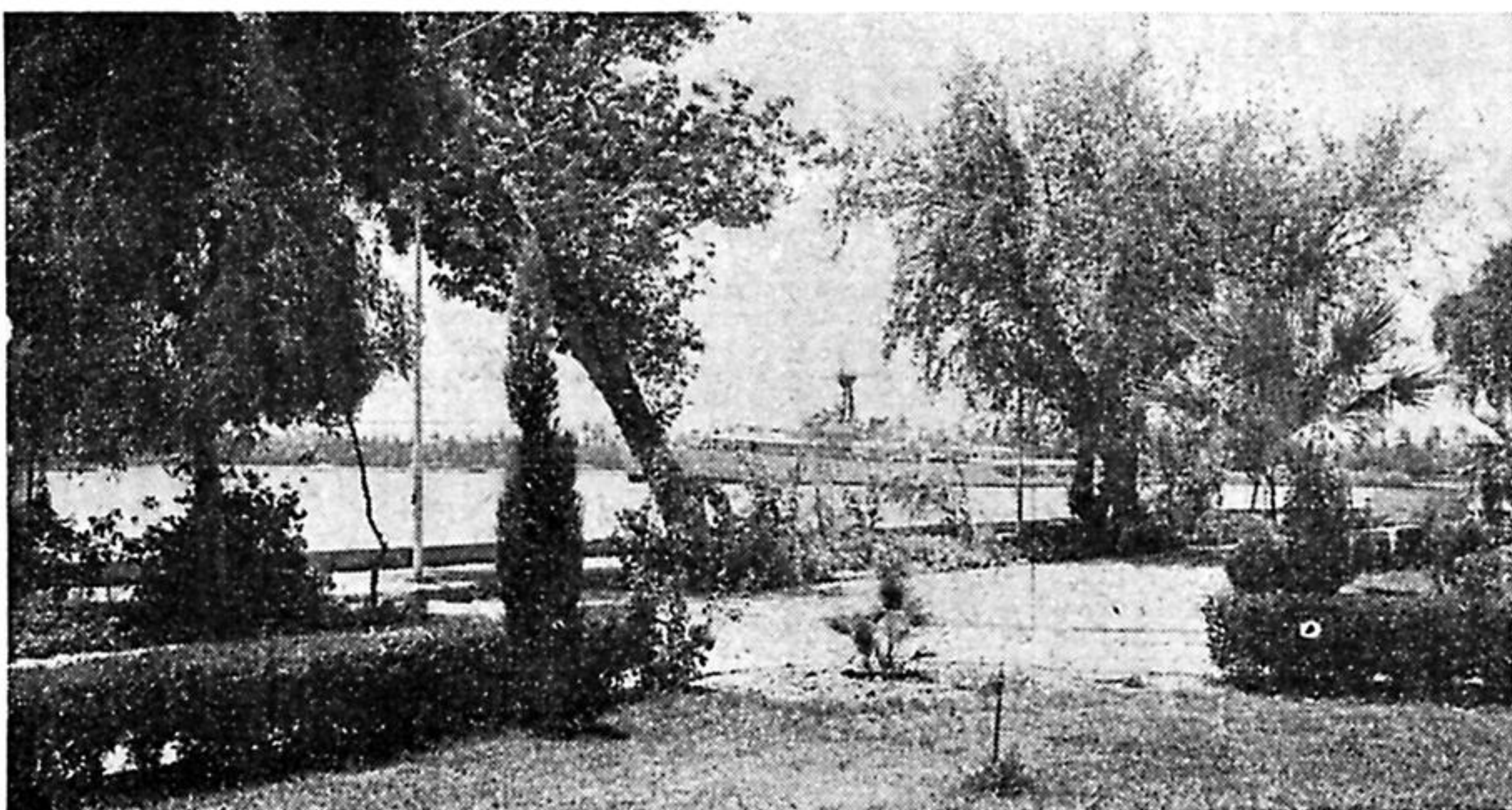
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Please send details of the Progressive Savings Scheme

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Address

Rating or Rank..... Age next birthday.....



H.M.S. Loch Fyne at anchor in Shat el Arab, off Khorramshahr Custom House—scene of the Queen's Birthday Parade

PERSIAN GULF FRIGATES VISIT KHORRAMSHAHR

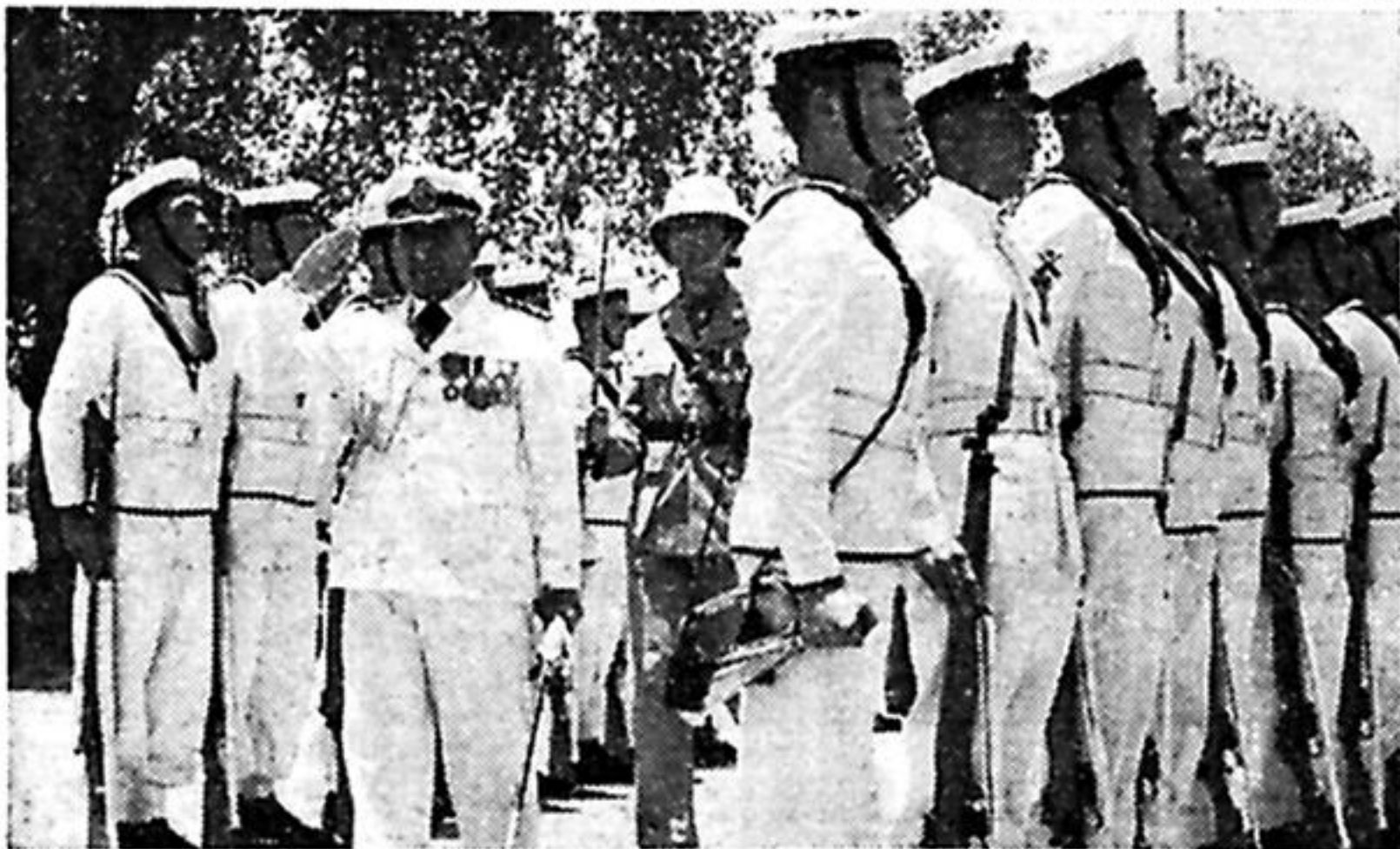
THE Flag Officer Middle East (Rear-Admiral A. A. F. Talbot, D.S.O.) visited Abadan and Khorramshahr from June 8 to 12, flying his flag in H.M.S. Loch Fyne (Commander P. J. M. Shevlin, R.N.). H.M.S. Loch Alvie (Commander J. H. Nethersole, R.N.) was in company.

The highlight of the visit was the

Queen's Birthday Parade which, to suit the Iranian authorities was held on Sunday, June 11. It took place in Khorramshahr, which is the main naval base in Iran, and the parade was formed of R.N. and R.M. contingents from the two frigates. The frigates themselves were moored in the stream in sight of the parade ground and fired

a co-ordinated 21 gun salute. The band of the Imperial Iranian Navy took part, and the parade was inspected by an Iranian Officer, Rear-Admiral M. Ardalan, C.V.O. Admiral Ardalan wore his Victorian Order, presented to him by the Queen during her recent visit to Iran.

The full sporting programme included, in addition to the normal sporting events, a forenoon of Go-Kart racing. During these few days, many friendships formed during previous Anglo-Iranian naval exercises were renewed and new ones made and, as part of the return entertainment offered by the ships. The Loch Alvie's five-piece skiffle jazz band made an appearance at a gala evening.



Rear-Admiral M. Ardalan, C.V.O., Imperial Iranian Navy, inspecting the combined Royal Navy and Royal Marine Guard drawn from H.M. Ships Loch Fyne and Loch Alvie



A GUINNESS IN EVERY PORT

All the nice girls
Love one, sailor.
And the sailor
Loves one, too!

GUINNESS FOR STRENGTH

G.E. 3549

H.M.S. JEWEL TO PAY OFF

H.M.S. JEWEL (Commander H. P. Hannion, Royal Navy), the senior ship of the Second Dartmouth Training Squadron, is to pay off in August. Jewel, an Algerine class Ocean Minesweeper, of 940 tons, and length 225 ft., is primarily engaged with the sea training of Cadets and Midshipmen from the Britannia Royal Naval College, Dartmouth, and on this duty, during July she visited Guernsey, Amsterdam, the Scilly Isles, and Bantry Bay.

The present Jewel is the fourth of her name to serve with the Royal Navy, the first being an armed merchantman of 110 tons and a complement of 60 men and which took part in the Battle of the Armada—hence her Battle Honour.

The second Jewel was a frigate of 40 guns built in 1809 and the third was a Drifter built in 1940 and mined on May 18, 1941.

The complement of the present Jewel is 95.

NEW HELICOPTER SQUADRON COMMISSIONED

A NEW Royal Naval Air Squadron commissioned at Portland on July 11 with four different types of helicopters. It is 771 Helicopter Training and Trials Squadron, which is the first in the Royal Navy to be specifically commissioned and equipped for a training and trials task.

The squadron is commanded by Lieutenant-Commander A. I. R. Shaw, M.B.E., A.F.R.Ae.S., R.N., and equipped with Wessex, Whirlwind, Dragonfly and P.531 helicopters.

Attending the commissioning ceremony at the R.N. Helicopter Station, Portland, was the Flag Officer Sea Training (Vice-Admiral P. W. Gretton, C.B., D.S.O. and two Bars, O.B.E., D.S.C.), Flag Officer Flying Training (Rear-Admiral F. H. E. Hopkins, C.B., D.S.O., D.S.C.) and the Captain of H.M.S. Osprey, Portland (Captain G. T. S. Gray, D.S.C., R.N.). Approximately 600 officers and men of the base maintenance and supply staff and H.M.S. Osprey were on parade with the Royal Marine Band, Portsmouth.

H.M.S. Scarborough (Cdr P. W. Buchanan, R.N.) recommissioned on June 6 at Portsmouth for service with the Fifth Frigate Squadron.

CEREMONIAL FIJIAN WELCOME FOR H.M.S. ANCHORITE

AFTER leaving Auckland on completion of her period of duty on the New Zealand Station, H.M.S. Anchorite (Lt. Cdr. G. G. W. Hayhoe, R.N.) visited Suva, from June 7 to the 12.

She berthed at the Kings Wharf watched by a large crowd of islanders, and as soon as the plank was over, four Fijians carrying a large kava bowl came on board and the traditional and ceremonial Fijian welcome of the coconut shell cupful of kava (a drink resembling dishwater in taste and appearance), was presented to the Captain and then to all on board.

On Saturday, June 10, being the Queen's official birthday, the annual birthday review took place, Anchorite contributing a platoon of 29 men. Only short notice had been received of this commitment—not an everyday one for a submarine, and it had been only possible to have two practices before arrival—particularly difficult in view of the complicated drill movements, including a feu de joie.

Things were made more difficult at the first rehearsal in Suva when it was discovered that the Fiji Military Forces did their drill to a completely different timing to the Navy. However, on the day everything went off perfectly and many compliments were received about the drill of the naval platoon which was very gratifying as the Fijians take their own drill very seriously.

ENTHUSIASTIC FIJIAN

On completion of the review, where the salute was taken by the Acting Governor, Mr. P. C. MacDonald, C.M.G., Anchorite's platoon marched back to the ship, led by the band of the Fiji Police Force and followed by an ever-growing crowd of enthusiastic Fijians, estimated at 3,000 by the time the ship was reached.

While alongside, officers and ship's company were entertained very liberally by the inhabitants of Suva, both white and Fijian, and the non-duty watches were able to live ashore at the R.N.Z.A.F. Station at Lauthala Bay, which was very welcome as the

humidity made life on board very hot and sticky. The flying boat base at Lauthala Bay must be one of the most pleasantly situated in the world, and led to several requests to transfer to the R.N.Z.A.F.

Sunday, the last day of the visit was spent by most relaxing on the sandy beaches around Suva.

FORTH VISITS HAMBURG

H.M.S. FORTH (Capt. M. L. C. Crawford, D.S.C.), Depot Ship of the Second Submarine Squadron based at Devonport, and two of her submarines, H.M.S. Alcide (Lieut. Cdr. P. N. Hamilton-Jones) and H.M.S. Amphion (Lieut. Cdr. J. L. S. Beauchamp), paid a courtesy visit to Hamburg from June 12 to 19.

The three ships were met by a large, enthusiastic crowd of Hamburgers, together with a German Army band, which provided "music while you berth!" The enthusiasm of the local population was shown positively when large crowds invaded the Depot ship and the "U-Boats" on the two open days that were held.

Formal ceremonies included the laying of a wreath by Capt. Crawford on the 1914-18 War Memorial.

A warm hand of friendship was extended towards the ships' companies by the local population: many ratings were invited to dinner by German families.

Tours, entirely arranged by the Germans, included the world-famous Hagenbeck's zoo, the ever-popular brewery runs, an oil refinery, a ship construction research station and, of course, Hamburg itself and outlying districts.

These visits, together with many others, succeeded in making the Second Submarine Squadron's short stay in Germany a happy and memorable one, and there were many heavy hearts when finally the band played "mus ich den."

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THE ROYAL NAVAL ASSOCIATION

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Patron: H.M. The Queen

CHEAM THANKS EASTBOURNE FOR A WARM WELCOME

THE Cheam and Worcester Park Branch of the Royal Naval Association reports that it is sailing along on a steady course, maintaining and even increasing its membership. With the aid of the local press the Association is getting quite a bit of publicity.

The summer dance was a very enjoyable affair and once again the Eastbourne branch gave the Cheam ship-

mates and their friends a very warm welcome on the Annual Outing. Cheam wishes to thank Eastbourne, publicly, for all their kindness.

The Stag Outing followed closely on the Annual Outing and those members who managed to get a late pass had a thoroughly enjoyable evening.

The No. 2 Area Chairman and Shipmate Asprey recently paid a visit to the branch. These two have undertaken rather a formidable task—they are endeavouring to visit each branch in the Area. They are both doing a good job both for the Area and for the Association and members at Cheam wish them well in their endeavours to increase membership and to foster the aims of the Association. There was a full muster of shipmates for the meet-

ing which lasted almost three hours. Constructive questions were asked and Shipmate Bates gave sincere and constructive answers and good advice.

Shipmate Connolly was the delegate for the Annual Conference and did a good job on behalf of the branch. The branch had submitted eight motions and had five accepted on the agenda for discussion. Although Cheam and Worcester Park may not have achieved what it set out to do, there is always another conference and if the branch plugs away it will eventually win.

Preliminary plans are being laid for the branch's Silver Jubilee in 1963. It is a long way ahead but the branch is determined to ensure that it has a perfect year.

NEWCASTLE READY FOR DEDICATION

THE next social event on the Newcastle and Gateshead branch of the Royal Naval Association calendar is the Annual Trip—this year to Bam-borough, organised by Shipmate Robinson.

The main item, however, during the next months is the Dedication of the Standard on October 22 at Newcastle Cathedral. A Royal Marine Band will be in attendance and the reception will be held in the Yeomanry Drill Hall, Northumberland Road. It is hoped that Admiral Hutton, the Area President, will take the salute.

The Newcastle and Gateshead branch has its first ex-Wren members, Shipmate Marie Hay and Shipmate Vera Campbell. The members are hoping that these two are the start of an influx of ex-Wrens as, according to one male member who shall be nameless, "our branch could do with a little feminine influence."

Shipmate Lackford attended the Annual Conference in company with the vice-president, Shipmate Lieutenant-Commander Anderson, R.N.R., and the members have been given a very lucid account of all the proceedings.

The Newcastle members think it is a pity that the next meeting could not be held in Scotland, but they realise that the expense would be too great.

The branch attended Dedication at Seaham Harbour and Consett and sends its thanks for two wonderful occasions. The secretary of the Consett branch is thanked for his advice regarding the expenditure, etc., likely to be incurred when Newcastle holds its Dedication. It is hoped that all No. 11 Area Standards will be in attendance on October 22.

Newcastle shipmates who took part in Consett's parade are asking if the march took them past the home of every Consett shipmate?

Shipmate A. Thirwell, secretary of the branch, has been honoured for his services and was recently made a life member of the Association.

BLACKPOOL HAS NEW H.Q.

THE Blackpool Branch of the Royal Naval Association has moved its headquarters to the Talbot Hotel, Talbot Road, Blackpool (opposite Blackpool North Station), where it meets each first and third Wednesday in every month.

During the period of the world-famous Blackpool illuminations, the branch intends to hold a social evening in the new headquarters every Friday and Saturday evening, when members will be pleased to meet members from visiting branches.

If any ex-Navy men should be in Blackpool during this period the Blackpool members cordially invite them, and their wives or girl friends, to join them.

Malta branch is still active

It is a long time since "Navy News" has been able to publish anything about the Malta branch of the Royal Naval Association, but thanks to the secretary of No. 1 Area, Shipmate E. Pledger, who has sent the Editor a letter from Shipmate J. A. Windley, secretary of the Malta branch, it is pleasing to record that the Malta branch is still in existence.

In his letter to Shipmate Pledger, Shipmate Windley says: "This is, I think, the first time the Malta branch and No. 1 Area have corresponded, so perhaps a little news of our branch would not come amiss. We are one of the newer branches, being inaugurated in 1958 by Captain The Earl Roden."

"Our numbers since that time have fluctuated from high to low—at the moment low—but increasing. Of course reductions in the Mediterranean Fleet and establishments do not help

matters, but although we are a small branch we still remain very active.

"The great majority of our members are still serving, so that you can imagine we see many new faces and say good-bye to many old ones."

"We extend a cordial welcome to any members who may get a draft chit to the Island. Meetings are held at the Brim Bar, Melita Junction, Sliema, on the first Wednesday of each month. Incidentally the Brim Bar is run by Shipmate 'Alf' Kingston, an ex-Chief Stoker, a mine of information on local conditions and well worth a visit."

"Our summer season of entertainments is just getting under way. There is a full list of beach trips, barbecues, tours and so on. There is no fear of the weather-man disorganising us—we get plenty of sunshine out here as you know. It makes things so much easier on the social side when you can always rely on the weather."

"Please convey to all members the best wishes of the Malta branch. Come and meet us if any of you are in the Island."

Bridport men go to sea

A FRIENDLY gesture by the commanding officer of the Inshore Minesweeper, H.M.S. Yaxham, Lieutenant Langton, Royal Navy, presented members of the Bridport branch of the Royal Naval Association with an opportunity to go to sea again—an opportunity of which several availed themselves.

Yaxham, based at Portland, was about to visit Bridport, and the Captain invited members to take passage from Portland to Bridport in the ship.

Together with a party of Sea Scouts the Bridport members sailed round to Bridport on the Friday and the next day played a friendly cricket match with Yaxham's crew. An eight-wicket win showed that the restricted space of Yaxham had not given the Yaxham's crew paralysis.

After the match the Yaxham crew were entertained to tea in the Bridport Bowling Pavilion—a tea organised by the lady members of the Bridport branch assisted by the branch secretary, Shipmate W. R. N. Cast.

Shipmate W. Perfitt, the branch chairman, welcomed the visitors and thanked the commanding officer for his co-operation in making such a memorable week-end. In his reply Lieutenant Langton said how delighted he was that everyone had thoroughly enjoyed themselves.

The same evening the ship challenged the Bridport members at skittles and Yaxham won again—by 11 pins. After the game the Mayor of Bridport visited the alley and was introduced to the ship's team.

During the return trip to Portland, members once again were guests of the commanding officer—a very fitting climax to an excellent week-end and for which the Bridport shipmates offer their grateful thanks to all in Yaxham.

BATTERSEA ENTERTAINED DELEGATES

WHEN the Battersea branch of the Royal Naval Association found that no entertainment had been laid on for delegates attending the Annual Conference after it had completed its work, the branch secretary invited the delegates to the Royal Naval Association Club at 122 Battersea High Street.

Members rallied round and with the resident band in attendance and sandwiches "on the house," the evening passed quickly and pleasantly. This excellent finale to the conference ended at midnight.

Delegates from as far north as Arbroath and as far west as Plymouth arrived, and so did the Northern Ireland and Eirean delegates.

Shipmate Frank Wade, the National Association Chairman dropped in and addressed the lively company.

There is always a great welcome at Battersea for all members of the Association who may be passing through London.

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Member of the Royal Naval Assn.
(Dartford Branch)

From the Editor's Chair

Are the branch scribes on holiday?

WHAT has happened to the branch "scribes" these past few weeks? Are they all on holiday? Although it is realised that gardens need attention and the families clamour to be taken out and away on holidays I am sure that a lot of activity is taking place within the various branches. Visits here and there—summer excursions—and the like.

Very often, in themselves, such outings are not "news," but on these trips old friends are met—yarns are told—and experiences swapped. Some of this would make excellent reading I am sure.

The Editor has been on the sick list for five or six weeks, but is now back in harness. Secretaries and "scribes" may have found that their letters have not been receiving the usual attention and the Editor regrets any delays which may have occurred, but he hopes to be soon up to date.

It has just been realised that perhaps some of the scribes have been sick too—but surely not all of them. News of branches is wanted—don't leave your notes until a couple of days before publication date, but send articles along as soon after an event as possible.

I am disappointed when news from branches is scanty. Disappointed because I feel that branches are missing opportunities of spreading news about the Association. Although each branch is a separate entity we ought not to be solely parochial in our outlook. A good effort in the North read by branches in the South may start up a healthy spirit of competition which can do nothing but good.

Again, serving people reading about branches in their own towns, can become interested in the Association if they read of activities in "Navy News." And remember your new blood must come from the present serving man. It is astonishing to me the large number of men now serving who do not realise that they are eligible for membership of the Royal Naval Association.

I would like to take this opportunity of thanking all secretaries and others who write to the Editor and to wish them well. Carry on with your excellent work, but always at the back of your mind remember—"This would make a good story for 'Navy News'."

We will remember them

Shipmate Commander the Hon. Anthony Pleydell-Bouverie, D.S.C., R.N. (ret.). A founder member and President of Aldermaston branch. Died June 24, 1961.

Shipmate F. Miller, Hon. Member of Royal Leamington Spa branch. Died July 14, 1961.

MYSTERY PARCEL EXCITES MARITIME MUSEUM

A ROYAL Naval petty officer's uniform dating from the Crimean War, 1855, has been sent anonymously to the National Maritime Museum, providing the Museum with one of its most interesting historical uniform exhibits.

An unsigned note accompanying the uniform said it was a family treasure "worn by my grandfather," and it was hoped it might be of use to the museum. The only clue to the sender was a postmark on the parcel: "Knot-tisford Street, Bethnal Green."

Early uniforms of the Royal Navy are virtually unobtainable and the National Maritime Museum wishes it knew the name of the anonymous donor, not only to thank him for his gift but also to know the name of the original wearer.

The gift is of special interest as it dates from the time when the lower deck of the Royal Navy had no standard uniform (adopted in 1857) though a combination of fashion, the system of central buying of clothing by the Admiralty for sale to the men, and the desire of officers for smart ships all led to some uniformity.

INTERESTING COLLAR

The garments which the museum has now received are of white duck and consist of a "frock" and trousers. The frock of this date was made something like the present jumper, but was much fuller and was worn tucked into the top of the trousers. It has a blue collar and cuffs. The collar is ornamented by a single white line made of three pieces of narrow white tape sewn close together. This is of particular interest to the museum as the Royal Navy's familiar three separate lines of tape for the sailor's collar were adopted when the uniform was standardised. The fullness of the frock is shown by the chest mea-

Combined club for Worcester and Malvern?

DISCUSSIONS are taking place between the Worcester and Malvern branches for a combined meeting place. A site for a hut near the Crown Inn, Powick, provided by a Worcester member, Shipmate J. Steer, has been offered. Shipmate Steer stated that he would be prepared to run the hut for both branches.

The annual meeting of the Worcester branch took place at its headquarters, the Farriers Arms, Worcester, and those present included the president, Captain H. M. Spreckley and the vice-president, Lieutenant-Commander J. McA. F. Cassidy. The branch chairman, Shipmate W. C. Badger, was in the chair.

During the meeting the Trafalgar Day dinner details were agreed. The roast Herefordshire turkey on the menu would be supplied by a Worcester branch life member, Lieutenant-Commander E. Seccombe.

ALDERMASTON PRESIDENT DIES

THE Aldermaston Branch of the Royal Naval Association was shocked by the sudden death of its President, Cdr. The Hon. Anthony Pleydell-Bouverie, D.S.C., R.N. (retd.), on June 24.

Cdr. Pleydell-Bouverie, who was a founder member of the branch, had a distinguished war service, being the Fleet Signals Officer to Admiral Lord Cunningham. Later he commanded a destroyer on Arctic convoys to Russia. He became the Liaison Officer with the United States Navy in the Pacific before returning to the United Kingdom to command another destroyer for the D Day landings.

A memorial service was held at Aldermaston Church on July 9 which was attended by shipmates of the branch.

surement of 44 inches, while the waist band of the trousers is only 26 inches. The trousers are not bell-bottoms, as might have been expected, but are only 19 inches round the ankle.

Another feature of the uniform given to the museum is an embroidered crown and anchor badge, only 1½ in. high, worn on the left sleeve. This was adopted as the badge of a 1st Class Petty Officer in 1827 and transferred to 2nd Class Petty Officer in 1853.

An official of the museum said: "While the museum has had little difficulty in obtaining the full-dress uniform of flag officers from 1856 onwards (except for the short period 1901-1904, when there were three changes in three years) it is much more difficult to obtain the uniform of junior officers. When one comes to the dress of seaman it has proved almost unobtainable."

EXCITING EXHIBIT

"We would very much like to know who has given us this uniform. It is one of the most exciting uniform exhibits we have had and we would like to put the name of the wearer with the suit when we put it on show. Unfortunately we are still trying to find a seaman's hat for the early 1850's. We know it was a straw hat but with a higher crown and smaller brim than the one later adopted by the Navy. It has been impossible to find either a cap or hat for this period. If we can trace the donor of this uniform we might be able to find the hat which went with it, although this is not very likely."



Tamar Bridge, now rapidly approaching completion

New bridge will save time, money and temper

80-TON ROADWAY SECTIONS

(BY W. FARRELL)

NAVY men who have not visited the Plymouth area for the last two years will find a considerable difference when they do visit that district. For years a waiting time of often an hour or more has had to be endured by people wishing to cross the River Tamar by the Torpoint or Saltash Ferries, but the Saltash, Brunel or Albert Bridge now has a partner, the Tamar Bridge, rapidly nearing completion, which will obviate a loss of time, money and temper.

The Saltash Bridge, which carries a single line of rail traffic between Devon and Cornwall, was designed by the famous engineer Isambard Kingdom Brunel and was opened in 1859. It consists of two main spans each 455 ft. long reached by approach spans from the banks of the river. The bridge floor is suspended by elliptical hollow curved tubes 17 ft. in diameter. The central pier supporting these tubes was sunk in the river bed to a depth of 80 ft. below the water level. This bridge will still continue to carry the main line of rail traffic from Devon to Cornwall.

The new bridge will carry road traffic on a three-lane carriageway 33 ft. wide with a 6 ft. footpath on each side. Approval to build the bridge was obtained in February, 1959, and work on the bridge started in August, 1959. The terms of the contract stated that the bridge had to be capable of carrying traffic within two years, i.e., by August, 1961. Although bad weather during 1960 delayed work, the fine weather we have had this year has enabled the constructors to make up the leeway and they hope to meet the contract date.

SERIOUS ACCIDENTS

An average of 200 men at a time have worked on the bridge and it says much for the safety precautions taken that there has been only one serious accident resulting in loss of life. This

was caused by one of the workmen falling off the top of the bridge.

Several other lives were lost when a boat capsized while taking some workmen across the river at the end of their shift.

The bridge has been built on the suspension principle, the main span of 1,100 ft. being supported by two towers of reinforced concrete. These towers reach to 260 ft. above high water and the main span has a minimum clearance of 100 ft. Next come two side spans each 374 ft. long, connecting to two shorter towers known as side towers. Finally there are two short approach spans of 80 ft. and 65 ft.

The roadway of the bridge was built in sections weighing 80 tons each. The main span consists of 19 of these bolted together to form a rigid structure. These trusses, as they are called, were floated into position on a pontoon, lifted with block and tackle connected to a winch and finally attached to the main supporting cable by means of suspender ropes. The side spans were positioned by means of a travelling crane which lifted them from the banks of the river. The roadway itself, now being laid on these sections, consist of reinforced concrete slabs 6 inches thick.

MAIN TOWERS

The main towers, made of hollow reinforced concrete, stand on 30 ft. diameter mass concrete foundations

sunk into the river bed to a depth of 50 ft. below high water level. They are bedded in solid rock. The side towers are similar but are set in rock at ground level. The foundations were made as follows: Concrete chamber caissons were constructed with knife-edged bottoms. They were fitted with airtight roofs and compressed air inserted to force the water out. Workmen then entered the caissons through air locks. They dug the river bed from under the caissons causing them to gradually sink lower until they reached the required depth.

During this time the top portions were continually being built up to maintain the necessary amount of freeboard. The bottoms were then sealed, the whole caissons filled with concrete, and the main tower erected on top.

13½-TON ROPES

The main cables pass over the top of the towers and are anchored in solid concrete blocks embedded in rock. These are set at an angle of 35 degrees to the horizontal and are tapered, the top of the block being 12 ft. x 16 ft. and the rear 19 ft. x 30 ft. The heads of these blocks connect up with reinforced concrete chambers which serve three purposes, anchoring the cables, supporting the approach spans and forming an abutment for the embankment. The cables themselves consist of 31 in number 2½ inches diameter locked coil steel wire ropes. Each rope is 2,200 feet long and weighs 13½ tons. The ropes connecting the cables to the roadway are 2 inches diameter and are fixed to the cables by cast steel clamps. The bottom ends carry the stiffening trusses which carry the roadway and also resist the effects of wind on the suspended sections. This is effected by their depth, 16 feet, which prevents hogging and sagging. A system of bracing members is also built into the stiffening trusses. This acts as a helical spring and resists any tendency to bridge may have to twist.

The Tamar Bridge fills a long-needed purpose which all lovers of the West Country will appreciate.

The cruiser H.M.S. Tiger (Capt. P. W. W. Graham, D.S.C., R.N.), is acting as Guard Ship at Cowes, July 29 to August 4, for Cowes Royal Regatta.

The first of two new fleet replenishment tankers was laid down at the Hebburn-on-Tyne shipyard of Hawthorn Leslie (Shipbuilders) Ltd. on July 24. When it is subsequently launched it will be named Tidespring.

Birthday cake flown in by helicopter

Jottings from Yeovilton

THERE was a birthday atmosphere on the station's Air Day at R.N.A.S. Yeovilton on June 17, for this year's event coincided with the 21st anniversary of the station's opening.

And just to make sure that this was not forgotten a huge imitation birthday cake was flown in by helicopter after the opening ceremony had been performed by the Commander-in-Chief, Portsmouth, Admiral Sir Manley Power.

Unfortunately the day was marred by a low cloud base, but a modified flying display was staged by naval aircraft—ancient and modern. Added to this was a parachute drop and an extensive static display which included an R.A.F. V-bomber.

On completion of the flying display the combined bands of R.N.A.S. Brawdy and Yeovilton beat "Retreat."

WRENS ON WAY TO STARDOM?

Wrens serving at the station have recently starred in two films. The first was "Girls Ahoy"—one of the "Look at Life" series. Many of the scenes in this film, which is concerned solely with the Women's Royal Naval Service were shot at the Air Station earlier this

year. When the film was shown locally, First Officer J. Witney, W.R.N.S. led 83 of the girls under her command to the Odeon, Yeovil, to see the finished product, as guests of the manager (Mr. J. E. Brown).

SPORTS DAY

Number 899 Naval Air Squadron narrowly beat the Air Electrical Division at the annual sports day held on May 9. In the men's events, Air Artificer Gibbs of the Air Engineering Division completely dominated the sprints, winning the 100, 200 and 440 yards in comfortable fashion. Wren C. Kay ensured victory for the Air in the W.R.N.S. events. She won all four events in which she entered.

Prizes were presented by Mrs. F. H. E. Hopkins, wife of Rear-Admiral F. H. E. Hopkins, the Flag Officer Flying Training. Music was provided by the Station Volunteer Band.

MOTOR-CYCLE SCHOOL

Proficiency certificates were awarded to the three ratings in the accompanying picture after they had completed a 12-week training course at H.M.S. Heron. They are Leading Naval Airman Pugh, Naval Airman Ruddy and Naval Airman Short.

Also in the picture are Sub-Lieut. Duffie, the training organiser, two instructors, and Mr. F. Turner (R.A.C./A.C.U. Executive), Mr. G. Draper (R.A.C. Driving Examiner) and Mr. Stocks (R.A.C. Superintendent).

[These notes were received too late for inclusion in the July issue of the paper.]



Three motor-cycle riders who were recently awarded proficiency certificates after a twelve-week training course.

Seen and heard on the beat

AS may well be imagined members of Naval Patrols see and hear many strange things in the course of their duties. The following incidents, all true, took place in either Plymouth or Devonport, and were contributed by members of the City Patrol, Plymouth.

A Leading Patrolman, having occasion to order a rating to put his cap on straight, was told: "I can't, my hair is too long."

A New Entry spoken to by a Leading Patrolman replied: "Can't you read? H.M.S. Drake on your cap, H.M.S. Raleigh on mine nothing to do with you."

A Naval rating on his hands and knees counting holes in a wire net fence demanded to know if there was

a law against it when brought to Headquarters for being drunk.

A Leading Patrolman on the beat felt ill when a rating he spoke to on Union Street pulled from his trouser pocket an unwrapped, greasy pig's trotter, covered in sluff and tobacco dust and commenced to eat it.

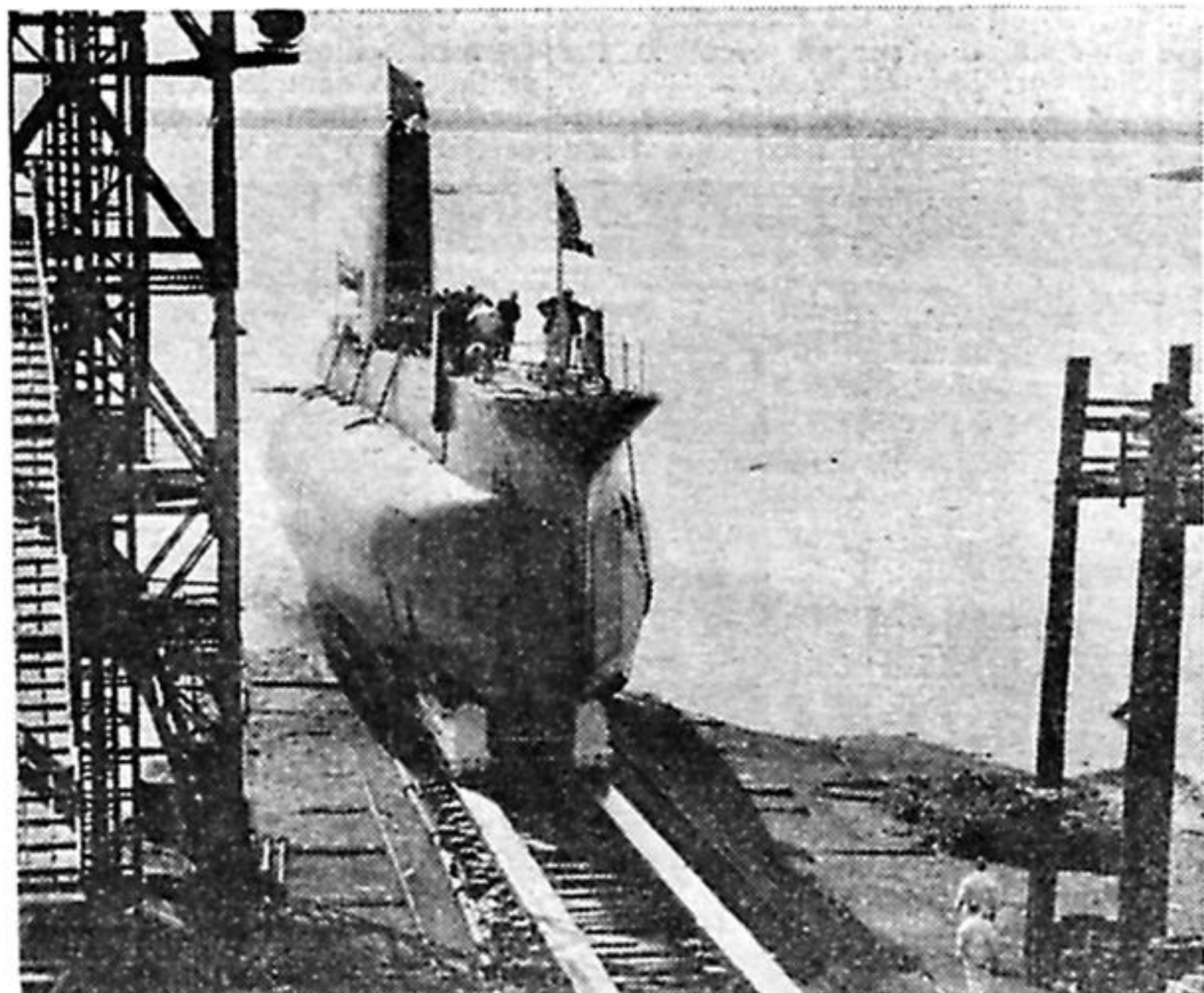
A Leading Patrolman on duty in the R.T.O. at Plymouth Station received a telephone call from an Army Officer wishing to order a flat. After being informed that the Naval Patrol did everything except deal in houses the Army Officer made it known that he required a "flat" railway truck.

Seen on Union Street, a Naval rating somewhat under the influence, pulling along one of his shoes on the end of a piece of string trying to convince passers by that it was a dog. Remarks by onlookers had to be heard to be appreciated.

A number of Naval ratings one Christmas won a duck in a raffle. The duck, being alive, constituted a problem none of the ratings were willing to solve by drastic measures. The duck finished the run ashore accompanying the ratings on the end of a lead.

An R.P.O. on duty on Plymouth Station was approached by a civilian who looked at his N.P. armband and said: "Oh wrong bank, I'm Barclays not National Provincial."

INTO HER ELEMENT



The sixth submarine of the Oberon class—H.M.S. Olympus—entering the water at Barrow, on June 14, after her launching and naming by Mrs. R. H. Wright, wife of Vice-Admiral R. H. Wright, C.B., D.S.C., the Flag Officer, Scotland

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Rear-Admiral G. B. Teale, C.B.E., presents the magnificent Dupree cup to App. Meakin, captain of H.M.S. Collingwood's team

Keen competition in Command sports

ONCE again the weather favoured the Portsmouth Command's big athletics day on July 4. In beautiful sunshine something like 200 officers and ratings and 50 Wrens, from approximately 20 different establishments competed against each other, not only to prove that their establishment was the "top dog" of Command athletics, but also for a place in the Portsmouth Command Team for the Royal Navy Championships.

The competition was keen and it is a credit to the athletes concerned when it is realised that the first three competitors in each event invariably bettered the winning time, etc., of other Commands in the Navy.

Special mention must be made of a few of the athletes. First, Steward R. Meadows, who not only bettered Lieutenant R. Pape's six-mile record by 19.4 secs. with a time of 30 mins. 37.6 secs., but also gained a hat trick in the middle distance events, winning the one, three and six miles races.

Next Leading Seaman G. Moralle who, for quite a few years, has been fighting hard to win a Command Winner's medal. This year he succeeded, not only in the Command Championships, but also, a week later, in the Royal Navy Championships. His time of 9 mins. 44 secs. for the 3,000 metres was by no means a record, but it was a creditable and pleasing result from a keen and fine athlete.

Moving to the Wrens' events, the outstanding athlete was Third Officer Hewitt-Taylor, who continued her prowess of previous years by winning

the 100 yards, the 220 yards, the 80 metres hurdles and the long jump. Wren Burge again increased her R.N. Javelin record with a throw of 111ft. 10ins.—an increase of approximately 8ft.

Generally results were good and when 27 A.A.A. Certificates are achieved (1 Grade One, 1 Grade Two and 25 Grade Three), surely the standard must be good.

The Inter-Establishment Trophy was retained by H.M.S. Collingwood with 111 points, H.M.S. Victory (68 points) being second and H.M.S. Excellent (47 points) was third.

The Minor Establishments Cup was won by H.M.S. Dryad (33 points), with H.M.S. Ganges (31 points) second and H.M.S. Pembroke (22 points) third.

The W.R.N.S. Inter-Establishment Trophy was retained by H.M.S. Excellent with 50 points, but H.M.S. Dauntless was a close second with 49 points.

The W.R.N.S. 4 x 110 yards Challenge Cup was won by H.M.S. Excellent, with H.M.S. President second and H.M.S. Dauntless third.

Grace and determination



Third Officer Hewitt-Taylor W.R.N.S., H.M.S. Dauntless, winning the 220 yards at the Portsmouth Command Sports

GAINED QUEEN'S COMMENDATION



Lieut. M. R. Hare, Royal Navy, and L./S. L. Cromwell, both of H.M.S. Caprice, who both gained the Queen's commendation for their part in the South China Sea rescue of the crew of the Panamanian ship, Galatea, in surf, in which the citation states "an error of judgment or a moment's hesitation could have meant disaster."

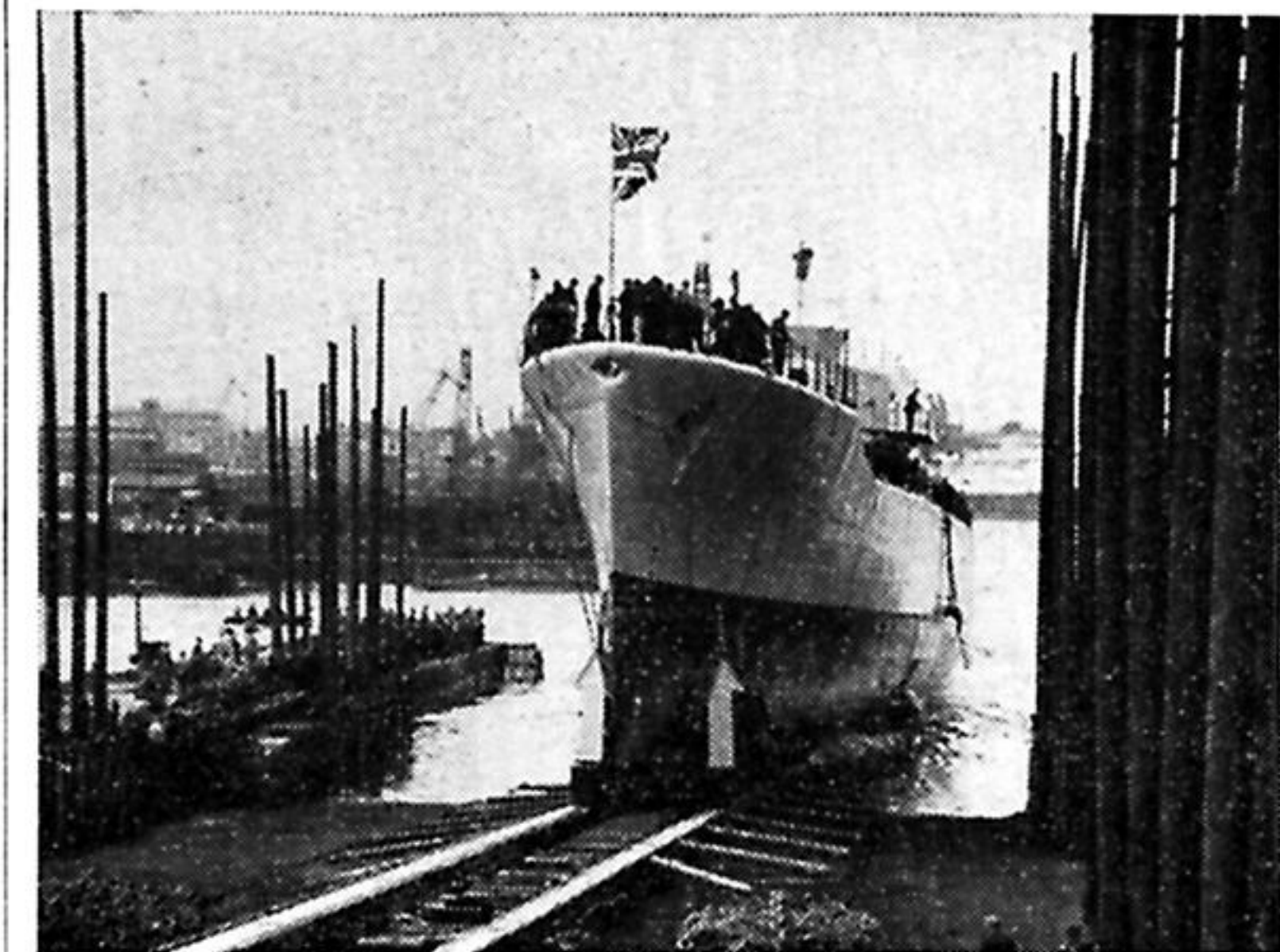
WITH THE W.R.N.S.

The W.R.N.S. athletics team distinguished themselves at the Inter-Establishment Meeting at Pitt Street on July 6 when they won the W.R.N.S. inter-unit challenge cup and the W.R.N.S. relay cup. This is the culmination of much hard work, and being one of the smallest units, the Wrens of H.M.S. Excellent can feel justly proud of themselves.

Wren J. H. Coates, Writer (Pay), H.M.S. Dryad, taking part in a swimming match between the Portsmouth Command Swimming Club and Worthing Swimming Club, won the 100 yards back-stroke in 77 seconds.

The W.R.N.S. Unit of the Depot, R.M., Deal, took part in a march through Deal at the end of a week celebrating the centenary of the depot, and showed the changes of uniform for the Royal Marines from 1861 to 1961.

A new frigate launched



As reported in the July issue, H.M.S. Leander, first of the new Leander class general purpose frigates, was launched on June 28. She is seen here entering the water at Belfast

Three thousand in Singapore walking race

NAVAL MEN IN FIRST 250

IT is 0600 on Sunday, April 23, 1961, and the scene is a rough piece of land at the 15th milestone, Woodlands Road. This is the setting of the sixth annual "Free Press" walk, a race of 14½ miles, sponsored by the "Free Press," a daily Singapore newspaper. The competitors are split up into four sections: the women, veterans, juniors and seniors, numbering about 3,000 altogether.

At 0630 the Veterans are off, striding out as though still in their youth. The women were gaily dressed in a colourful selection of shorts, skirts and dresses, one lady, the eventual winner, being in a two-piece which looked very cool.

Next the Juniors, and what chaos! Those at the back of the field were so keen to get going that they ran, trampling on those in front trying to walk. As the milling mass of arms and legs gradually sorted itself out, we could see numerous odd plimsols, "flip-flops" and other footwear which had been lost at the start.

At about 0705, some 35 minutes after the Veterans had "got away," the Seniors left. There was a bit more organisation at the start, but nevertheless there was plenty of running by the leading "walkers."

Although the sun had just about risen, the morning was still quite cool, but with no sign of rain a warm, or rather hot, morning's walk was to be expected. The pace was quite fast for the first two or three miles while the field gradually ended its initial sprint for a good position at the start. The leaders in the Senior and Junior sections began to make determined efforts to close the gap between themselves and the leaders of the Women and Veterans.

DENSE CROWDS

Milestones, when they could be seen through the dense crowds lining the roads, helped to keep the walkers informed of the distance covered and how much farther they had to go. It had meant an early rise for the thick crowds all along the route who were witnessing the biggest event in the Singapore Sporting Year.

Drinks and chewing gum were provided for those in need of a thirst

quencher or something to keep their minds off walking.

By about 0830, the sun had begun to make its presence felt, and the heat became really intense, the temperature nearing the 100 mark. This, however, did not hold back the leading competitors who maintained a very fast pace throughout.

Police controlled all crossings and traffic was held up for the walkers, although many drivers showed their impatience by blowing their horns incessantly.

'COME ON JOHN'

Then came the most gruelling part of the race, the long stretch of road towards Newton Circus, about 6 miles of practically straight road. The crowds were very thick and the field could be seen for miles. Many local inhabitants were ready with a bucket of water to revive the tired walkers, and there were many shouts of "Come on John" as the English walkers came by.

Soon Newton Circus loomed ahead, a sign to the weary walkers that the end was near, and for those still with some energy left it was a sign to make up lost ground on tired walkers.

As Farrer Park came nearer the crowd became thicker, so thick that only one way traffic for the walkers was possible. The extremely tired walkers had now only to walk on to the arena at Farrer Park and be given their final position.

So ended, for many, weeks of hard practice, and for the rest, stiff muscles for competing for the fun of it, or merely to say "I did it."

It was quite an achievement for a team of "sailors," and "Office" sailors at that, to complete the course, and for two of them to be in the first 250 of the 1,600 Seniors.



The "office sailors" team: L./Wtr. B. Tooes, of Portsmouth; L./Wtr. C. Gregory, of Saltash; L./Wtr. D. J. Downes, of Nottingham; Wtr. C. Kay, of Middlesbrough; Wtr. P. Danks, of Woking; Wtr. R. Boulton, of Ellenhall; and one other

'Button Boy's' big thrill

SIXTEEN - YEAR - OLD Leslie Smith, of Church Road, Aldershot, was on top of the world—well, almost. He was picked to be "button boy" out of nearly 1,800 boys at H.M.S.



Capt. J. R. Grower, R.N., and Junior Mechanic Engineer Leslie Smith

Ganges, Royal Naval training centre, and in front of the Queen he shinned up the 142 ft. high mast to stand right on the top. The button is only 18 in. across and all Leslie had to help him keep his balance was a metal spike which he gripped between his knees as he proudly saluted with the other 80 boys arranged on the mast.

Leslie only got the coveted job by chance. The boy who was to have had the "top job" of the year at Ganges, hurt his ankle four days earlier while climbing down a rope, so Leslie moved in.

After the mast-manning display he had an even bigger thrill—he was presented to the Queen and received a silver medal from her to commemorate his big day.

It was the second medal he has for being a button boy—the first was presented to him earlier in the month by Captain J. R. Gower, C.O. of H.M.S. Ganges, after Leslie shinned up the mast on parents' day.

Said Leslie's mother: "Of course we are all terribly proud of him—it's a great thrill and a great achievement."

"Leslie is very keen on his training at H.M.S. Ganges, and he is going to make the Navy his career. At the moment he is a leading junior mechanic engineer."

Leslie said recently that he was sorry for the original button boy who had to miss out on the big day—but he was pleased as punch to get the job himself.

Leslie's father, seriously ill in hospital, got a tremendous morale boost when he heard about his son. And rightly so, for Leslie has done a difficult and dangerous job well.

To the Smith's he is top of the poll—and top of the pole!

'Lau Fix, no trouble'

LAU KAN PUI, the ship's tailor of H.M.S. Victorious, was born in Hong Kong on May 22, 1936, being, according to Chinese custom, one year old at birth.

On completion of his schooling at a Chinese school in Hong Kong, as he put it "no English," he was apprenticed to a tailor at 15. The first months apparently were spent painstakingly learning to sew on odd scraps of waste material, nothing else but hours of practising: a straight line, a seam, a buttonhole, all to be accomplished without leaving a mark of any kind to show the material had even been handled.

Whilst still under apprenticeship he joined the tailors' shop aboard H.M.S. Unicorn and got his first taste of life aboard H.M. ships. The year, 1949. In 1953 he spent a year ashore at Hong Kong drumming up business from the ships in the dockyard and working incessantly to cover the resultant orders.



Lau Kan Pui, whose work is known throughout the Far Eastern Fleet

PROGRESSIVE

From there he joined H.M.S. Chichester as Number One Boy, a fully fledged tailor in charge of the shop. About a year later transferring to H.M.S. Belfast. Being Number One Boy was hardly enough so he became his own master and with a staff of three tailors joined H.M.S. Gambia for 14 months. When the Gambia paid off in U.K. he joined H.M.S. Victorious complete with staff.

By this time the orders grew and word was passed on from old ships for Lau to make suits, even if it meant waiting some weeks for the orders to be sent by post from one end of the world to the other.

With this expansion of business,

TO TOUR COUNTRY

TEN 4-ton vans of a novel type, which open out and display various aspects of naval life and career, have recently been specially built.

The Royal Navy Mobile Display Units will tour the United Kingdom, visiting towns and rural districts during fetes, on market days and on other suitable occasions.

This should be an interesting event for the friends and relations of those now serving and also those on leave, and for anybody interested in a naval career.

H.M.S. CAESAR RELIEVES CAVENDISH

H.M.S. Caesar (Captain H. A. Corbett, D.S.O., D.S.C., R.N.) arrived in Singapore recently to relieve H.M.S. Cavendish, as leader of the Eighth Destroyer Squadron.

She took exactly a month to steam the 8,000 miles to Singapore. This allowed two days leave in Gibraltar and stops of a few hours in Malta and Colombo.

On arrival on the station the ship spent a week in Singapore, then called at Hong Kong — that perfect run ashore — for two nights before joining the Far East Fleet in the middle of the Japanese Cruise.

For her first goodwill visit, H.M.S. Caesar spent four days at Tokuyama, a small but friendly port.

obviously a staff of three was not enough and at the time of writing there are now no less than 12 tailors working continuously.

Naturally, fired with ambition, tailoring is not enough, so his sights were set on other fields. In H.M.S. Victorious now, besides his tailoring business he has four cobblers and a barber, while to help out on other orders he has a factory in Hong Kong of 10 more tailors. If you should need a Chinese tailor shop, cobblers, barbers or a laundry crew for your ship, "Lau fix, no trouble."

AMBITIOUS OUTLOOK

When the present commission in H.M.S. Victorious ends he wants to go on working indefinitely for H.M. ships. He seems to have so many friends of all ranks in the Service that to leave would make him a stranger in the outside world.

His family still live in Hong Kong, but he is single and looks like staying that way for quite some time to come. His business is obvious, but his hobbies may seem a bit strange, according to him they are "making suits."

For a lad that is pushing the ripe old age of 25 this year, his business record seems quite good. What will the future look like if he carries on this way?



The Lynx berthed at Jacksonville, Florida, with a skyscraper background of the Atlantic Coast Line building

Big drummer had to use his fists

LYNX VISITS VENEZUELA

(BY A NAVY NEWS REPORTER)

SINCE the last report of the activities of H.M.S. Lynx appeared in NAVY NEWS, the ship has visited Venezuela for the celebration of that country's 150th anniversary of its independence.

After a short visit to Jacksonville, Florida, where the famous Southern hospitality was almost overwhelming,

Successful salvage recalled

H.M.S. Loch Ruthven (Capt. D. B. Law, M.B.E., D.S.C., R.N.) returns to Devonport next Wednesday and then to Rosyth on August 14, after one year spent in the Arabian Seas and Persian Gulf.

Loch Ruthven has steamed 35,000 miles since she was commissioned on June 11, 1960.

Noteworthy was the successful salvage, in company with other R.N. ships, of the 22,000-ton motor tanker Polyana, on fire in the Gulf, and the attempted salvage of the 5,000-ton cargo liner Dara, which sank within three miles of safety.

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STUBBINGTON, 1958 detached furnished two-bedroom bungalow. Everything as new. Neat garden. Garage, base, Conservatory. 13100. Owners emigrating. 31 Robins Close, Stubbington.

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EX-ERA, required to assist in the care and maintenance of various plant, including refrigerators, calorifiers, boilers, etc., able to carry out repair work and help in periodical examinations. Apply Personnel Officer, British Insulated Cables, Ltd., 38 Wood Lane, London, W.12.

VACANCIES for Constables exist in the Admiralty Constabulary. Initial appointment will be on a temporary basis with prospects of transfer to permanent and pensionable service. Commencing pay of entrants is £25 per annum, with nine annual increments of £25, making a total of £775. After 17 years' service a further increment of £25 is awarded, making a final total of £800 per annum. Uniform and boots are issued free of charge. There are good prospects of promotion. Candidates must be between 21 and 48 years of age, at least 5 ft. 7 in. in height (in bare feet), of British nationality and of exemplary character. They will be required to pass a medical and educational examination prior to appointment. Educational examinations are held quarterly at H.M. Dockyards Portsmouth, Devonport and Rosyth, and at the R.N.M.Y., Londonderry, Northern Ireland. The next is due in early October, 1961. Further information and application forms can be obtained from the Chief Constable, Admiralty Constabulary, Queen Anne's Mansions, St. James's Park, London, S.W.1. Serving personnel wishing to make application should do so through their Commanding Officer, in accordance with Admiralty Fleet Order 2060/60. **WANTED** a young man in regular employment to work a first class spare time agency. Excellent terms and scope for advancement. Write Box 102, "Navy News."

MISCELLANEOUS

C. H. BERNARD & SONS LTD. are always happy to consider purchasing second-hand swords with or without scabbards, and any Officers wishing to sell should write in the first instance to Anglia House, Harwich, Essex, or at the Naval Ports to contact the local branch.

FORBES SELF-DRIVE HIRE. Cars delivered alongside on ships' return to the United Kingdom at no extra charge. New Morris 1000s—comprehensive insurance. Special low rates. Apply c/o White Rose Engineering Company, Liss, Hants. Phone Liss 2121.

HOUSE PURCHASE. An ideal scheme for those looking ahead 100% advance after three years at low rate of interest. Mortgages repaid in the event of death.—Write for leaflet, "The House Assured," to the Provident Life Association of London Ltd., 246 Bishopsgate, London, E.C.2.

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A limited number of vacancies exist at Atomic Energy Establishment, Winfrith, Dorset for

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Ability to read drawings and circuit diagrams and to construct, wire and test electrically complex equipment is essential. A knowledge of transformers or armature winding and relay control circuits would be an advantage.

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To operate and maintain a wide range of electro/mechanical precision instruments. Experience of oscilloscopes, signal generators, amplifiers and other electronic instruments is desirable.

Interesting and varied work where there are opportunities for advancement. Assistance may be given towards further education.

The Authority has sick leave and superannuation schemes and is at present operating a local assistant travel scheme.

Certain married men living outside the Winfrith transport area may be eligible for housing and lodging allowance while awaiting allocation.

An explanatory booklet and application form can be obtained from The Labour Office, A.E.E. Winfrith, Dorchester, Dorset, quoting reference SK/NN.

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for the above Department. Experience in electronics is necessary and, whilst not essential, supervisory experience would be an advantage. In addition to the conditions of employment outlined above the successful applicant would receive full payment during illness and authorised absence. In both cases a tax-free subsistence allowance of £3 per week is paid to married men, £2 to single men, and financial assistance is available for house purchase. Applications in writing, stating age and full experience, should be addressed to the "Labour Officer" at the above address.

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Youth championship results augur well for future

ATHLETICS 'BUG' SHOULD BE ENCOURAGED

THE Royal Naval Youth Athletic Championships were held in H.M.S. Ganges on July 15 and there were some first-class results. The athletes themselves, their coaches and the organisers of the meeting deserve the highest praise for their endeavours.

Prior to the presentation of prizes, Captain J. R. Gower, D.S.C., Royal Navy, Chairman of the Royal Naval Amateur Athletics Association addressed the athletes as follows: This is our second year only at staging these Youth Championships. It has been a remarkable success and the improved performances over last year augur well for the future of Naval Athletics. Three weeks ago at Portsmouth we had the Royal Naval Junior Championships. Next year most of you will be Juniors in the athletic sense, and I rely on you to carry on the good work in the 1962 Championships unless you are at sea.

It is important for the future of Naval Athletics that the Royal Naval Youth and Junior Championships flourish, as it is the base of the athletic pyramid from which we build up to a pinnacle—the Navy athletic team. At long last it is realised that Athletics must be a full-time summer activity with a track available at all times from Easter Leave to Summer Leave; not just a corner of the playing fields borrowed from an unwilling cricket

fraternity a few days before the Annual Sports Day.

FIRST CLASS TRACK

Here we have a first class track built and maintained by our own resources without Admiralty finance or support. Believe it or not today is our 38th fully organised athletic meeting since the summer term started ten weeks ago. Only in this way, with encouragement, regular competition and reasonable facilities can the athletic "bug" thrive. A virile bug can quickly develop into a disease, which spreads and multiplies. Medically we normally want to curb a disease; athletically it is just the reverse.

Notable performances today have been in athletic order of merit: Brough (Ganges), Hammer, 176 ft. 9 in.; Brough (Ganges), Weight, 45 ft. 9 in.; Webb (Ganges), Pole Vault, 10 ft. 3 in.; Halifax (Ganges), Discus, 156 ft. 6 in.; Brown (Royal Marines), 220, 23.8 sec.; Jenkins (Ganges), 440, 54.5 sec.

Some of these are up to National Standard and some would have won a pre-war Navy Championship.

To what can one ascribe this success?

Firstly I would say the post-war upsurge and popularity of athletics throughout the country in the Secondary Modern and Grammar Schools from which most of you have been recruited.

Secondly your own natural ability and interest in athletics.

Thirdly good coaching. At Ganges we have to thank Petty Officer Dutton for his field event coaching; at Deal Corporal Haliday has been dynamic in his enthusiasm, not only for Junior Wing athletics, but for Corps athletics; at St. Vincent and at Fisgard, you have been lucky to have helping you Chief Petty Officer Lucas and Petty Officer Brown respectively.

I have spoken long enough to remind you of how vital your contribution can be to athletics and how it can be welded into a successful Naval Athletic Team from 1959 onwards when you become Seniors. As Chairman of the Royal Naval Amateur Athletic Association I look to you for your continued support in this great summer sport.

110 Yards Hurdles: 1, J.M.E. Curtis, Ganges, 15.8 (Championship record); 2, J.E.M. Herskowitz, St. Vincent, 16.0. **100 Yards:** 1, J./Mne. Brown, Royal Marines, 10.8; 2, J.E.M. Thompson, St. Vincent, 11.4. **220 Yards:** 1, J./Mne. Brown, Royal Marines, 23.8; 2, J.E.M. Thompson, St. Vincent, 24.7. **440 Yards:** 1, J.R.O. Jenkins, Ganges, 54.5 (Championship record); 2, J./Mne. Lawson, Royal Marines, 54.7. **880 Yards:** 1, J.M.E. McCarthy, Ganges, 2 min. 17.4 sec.; 2, J./Bugler Douglas, Royal Marines, 2 min. 19.6 sec. **One Mile:** 1, J.N.A.M. Dale, Ganges Ind., 4 min. 59 sec.; 2, J./Sea Mack, St. Vincent, 5 min. 2 sec.

1500 Metre Steeplechase: 1, J./Sea Ellis, St. Vincent, 4 min. 53.8 sec.; 2, J.M.E. Dennis, Ganges Ind., 4 min. 54 sec. **High Jump:** 1, J.E.M. Briers, St. Vincent, 5 ft. 1 in.; 2, J.M.E. Stevens, Ganges Ind., 5 ft. 1 in. **Long Jump:** 1, J./Sea Claydon, St. Vincent, 17 ft. 7 in.; 2, P.O. Jnr. Roles, Ganges, 17 ft. 6 in. **Hop, Step and Jump:** 1, J.E.M. Herskowitz, St. Vincent, 36 ft. 5 in.; 2, App. Morrison, Fisgard, 36 ft. 2 in. **Pole Vault:** 1, J.N.A.M. Webb, Ganges, 10 ft. 3 in.; 2, J./Mne. Morris, Royal Marines, 10 ft. 0 in. **Weight:** 1, P.O. Jnr. Brough, Ganges, 45 ft. 9 in. (Championship record); 2, J./Mne. Brown, Royal Marines, 45 ft. 21 in. **Hammer:** 1, P.O. Jnr. Brough, Ganges, 176 ft. 9 in. (Championship record); 2, J.E.M. Williams, St. Vincent, 172 ft. 8 in. **Discus:** 1, P.O. Jnr. Halifax, Ganges, 156 ft. 6 in. (Championship record); 2, J./Mne. John, Royal Marines, 142 ft. 0 in. **Javelin:** 1, J./Mne. Griffin, Royal Marines, 141 ft. 4 in.; 2, J.E.M. Weston, St. Vincent, 138 ft. 7 in.

Team Competition Points: 1st, H.M.S. Ganges, 73; 2nd, Royal Marines, 71; 3rd, H.M.S. St. Vincent, 70; 4th, H.M.S. Fisgard, 47; 5th, H.M.S. Dryad, 18.

In the Quadrangular Games between H.M.S. Ganges, H.M.S. Fisgard, H.M.S. St. Vincent and the Royal Marines, Ganges took first place in Athletics, Sailing, 22, Swimming and water polo, but had to be content with equal third place at cricket. Points were: Ganges 22, Fisgard 14, St. Vincent 14 and Royal Marines 9.

At athletics Ganges obtained 57 points, St. Vincent 50 points, Royal Marines 47 points and Fisgard 31 points. Runners-up to Ganges in the sailing races were St. Vincent second and Fisgard third. In the 22 shooting Ganges led with 579 points, St. Vincent having 574 points, Fisgard 554 and the Royal Marines 517.

At water polo Ganges won its three games, Fisgard won two and lost one, the Royal Marines won 1 and lost two and St. Vincent lost three.

At cricket Fisgard and the Royal Marines each won two matches and lost one and St. Vincent and Ganges each won one and lost two.

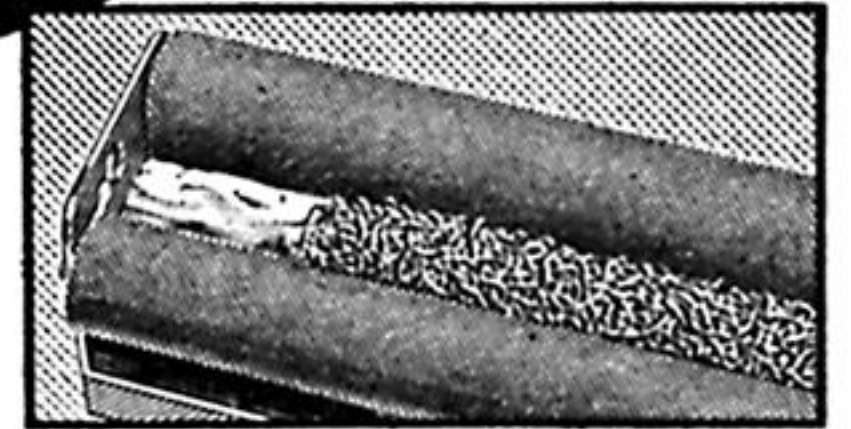
In the Swimming and Diving Championships, H.M.S. Ganges ran out easy winners. The points obtained were Ganges 122, Fisgard 85, St. Vincent 62 and the Royal Marines 54.

Diving, 1 metre springboard: J.R.O. Davies, Ganges, 16.47 points; E.M. Irvine, Dolphin, 14.73. **Diving, 5 metre firmboard:** J.R.O. Davies, Ganges, 15.16; E.M. Irvine, Dolphin, 13.65. **Swimming, 100 yards free-style:** J.M.E. Gorman, Ganges, 62.0; J./Sea Rhodes, R.N.B., Portsmouth, 64.6. **220 yards free-style:** J.R.O. Edwards, Ganges, 2 min. 42.8 sec.; J./Mne. Beaumont, Royal Marines, 2 min. 56.5 sec. **100 yards back-stroke:** J./Sea. Blower, Ganges, 73.0 sec.; J./Sea Wood, St. Vincent, 78.1. **661 yards butterfly:** J./Sea. Ainslie, Ganges, 50.9 sec.; J.M.E. Gorman, Ganges, 52 sec. **100 yards breast-stroke:** App. Withers, Collingwood, 81.9 sec.; App. Swinyard, Fisgard, 83.4 sec.

Team relay (6 by 661 yards): 1, Fisgard, 4 min. 7 sec.; 2, Royal Marines, 4 min. 17.2 sec.; 3, Ganges, 4 min. 14.4 sec.; 4, St. Vincent, 4 min. 21.9 sec. **Medley relay (661 yards back, breast, butterfly and free-style):** 1, Ganges, 3 min. 9.2 sec.; 2, Fisgard, 3 min. 21.4 sec.; 3, Royal Marines, 3 min. 34 sec.

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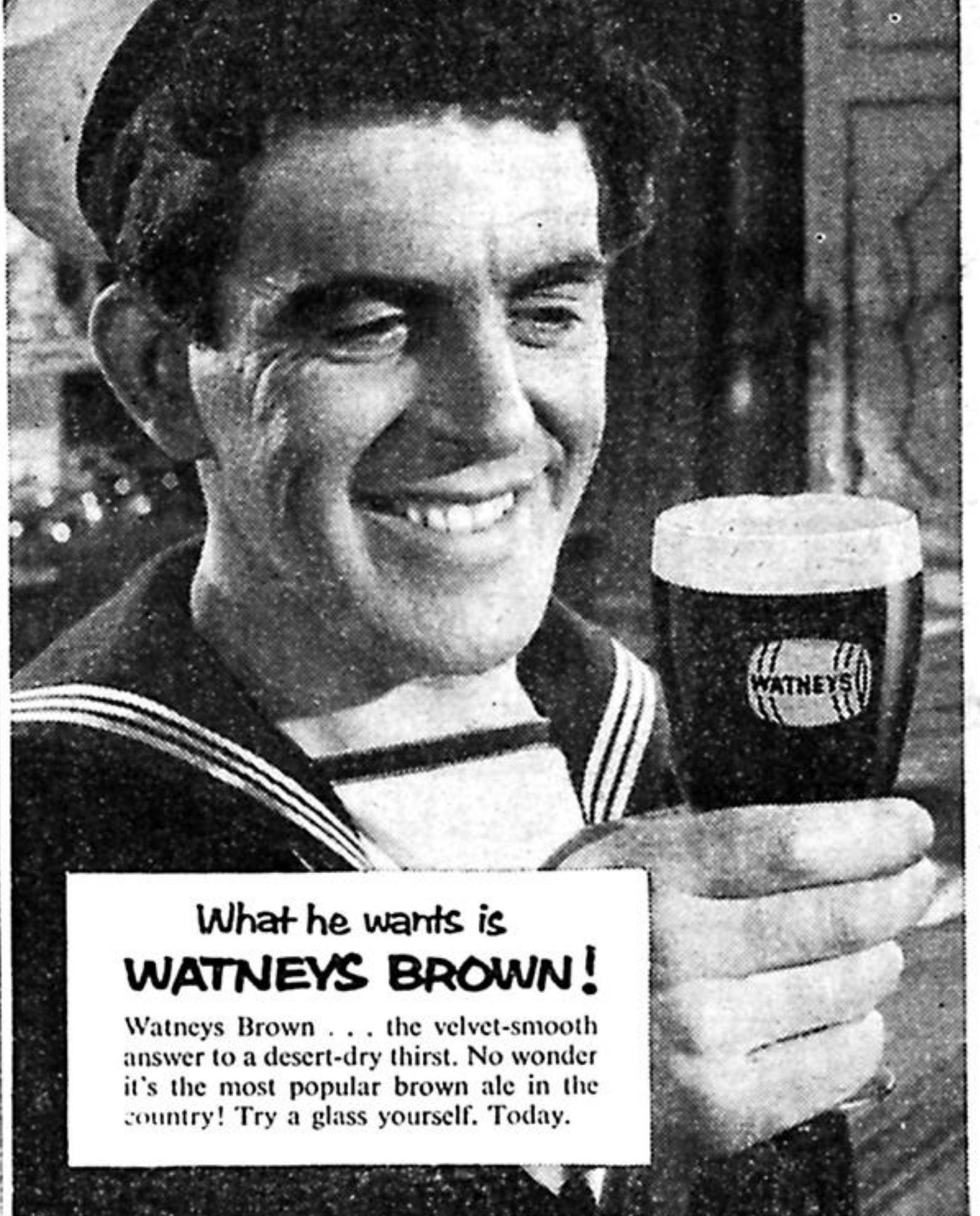
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A FUTURE CHAMPION



J.R.O. Spencer-Watkins, of H.M.S. Ganges, has, at the age of 16, proved that he has the makings of a future champion by winning the Royal Naval Cycling Association 50-mile mass start in a time of 2 hours 9 minutes 4 seconds. For the last eight miles he rode alone against the three members of the H.M.S. Ariel team and in the final sprint home proved himself the strongest of the four riders. Spencer-Watkins certainly justified his selection for the Navy team.

PORTSMOUTH WIN FOUR TROPHIES

THE result of the above Championships, held at the R.N. Athletic Club, Pitt Street, Portsmouth, on Tuesday, July 11, 1961, was as follows:

1 Portsmouth Cmd...	133 Pts.
2 Home Air Cmd...	120½ Pts.
3 Plymouth Cmd...	72 Pts.
4 R.M. Cmd...	58½ Pts.

W.R.N.S.:
1 Portsmouth Cmd... 77 Pts.
2 Home Air Cmd... 67 Pts.
3 Plymouth Cmd... 29 Pts.

All four trophies presented were won by Portsmouth Command.

The Gordon Wright Trophy presented for the 3 miles event was retained by Stwd Meadows, H.M.S. Gavinton.

The trophy presented for the W.R.N.S. 100 yards event was won by 3/0 Hewitt-Taylor, H.M.S. Dauntless.

LADY WON THE TREASURE HUNT

THE Plymouth group of the Forces Motoring Club held a treasure hunt on July 11, starting from the Home Park car park. There was the usual varied selection of cars and vans and for the first time there was a motorcycle.

The "treasures" were a stinging nettle, a wax night-light, a saucepan scourer and an obsolete blue pound note; the last item proved tricky since most have been withdrawn by the banks.

Three cars had equal points, so the one with the shortest mileage won the event—Miss J. Penney—a Plymouth hairdresser who is one of the club's regular guest competitors.

Officials cannot remember when an event was last won by a woman. Perhaps women drivers are not as bad as they have been painted. The winner had a lady navigator (or is it navigator) too.